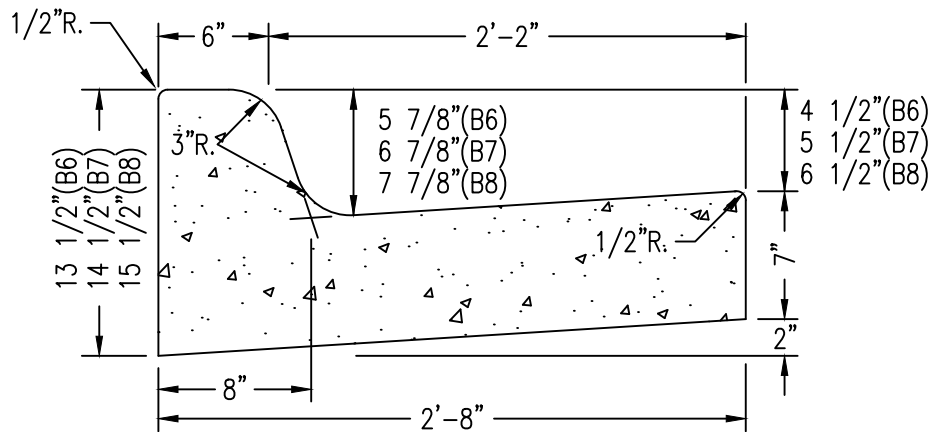


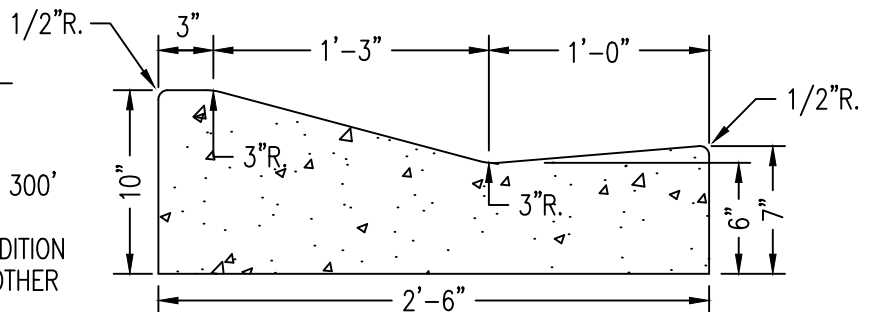
DESIGN "V"(VERTICAL)



DESIGN "B"(BATTERFACE)

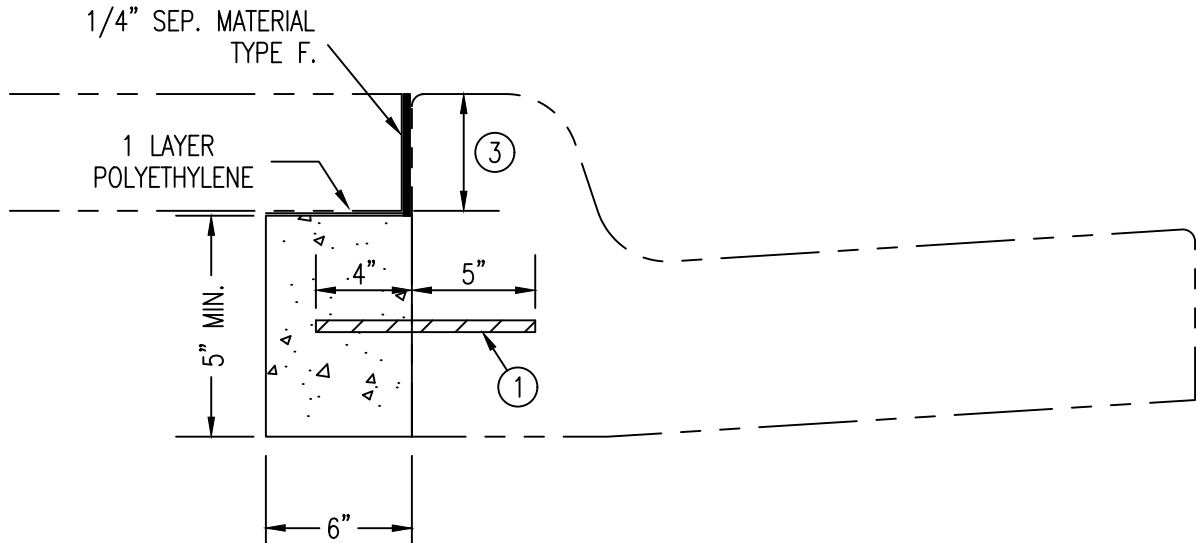
NOTES

1. CONCRETE MIX: MANUAL PLACEMENT-MN/DOT SPEC. 3F52, SLIP-FORM PLACEMENT-MN/DOT SPEC. 3F32.
2. PROVIDE 1/2" EXPANSION JOINT AT 300' MAXIMUM SPACING AND TO MATCH PAVEMENT EXPANSION JOINTS IN ADDITION TO EXPANSION JOINTS SHOWN ON OTHER DETAIL PLATES.
3. PROVIDE CONTRACTION JOINTS @ 9' MAXIMUM SPACING, SAW CUT 2" MINIMUM DEPTH.
4. ALL CONCRETE CURB & GUTTER SHALL BE PLACED ON A MINIMUM OF A 4" AGGREGATE BASE.
5. SILL REQUIRED FOR DESIGN "V" AND "B", WHEREVER SIDEWALK ABUTS CURB. SEE S.D.P. 2-02.



DESIGN "D"(DRIVEOVER)

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA			
CONCRETE CURB & GUTTER			
<i>Douglas E. Nelson</i> ASST. CITY ENGINEER		<i>Reed W. Frenn</i> DIRECTOR	
SHT 1 OF 1 SHTS	DATE REVISED 3/23/18	PLATE NO. 2-01	REV. G



MODIFIED B624 CURB & GUTTER
MODIFIED V624 CURB & GUTTER (SIMILAR)

NOTES

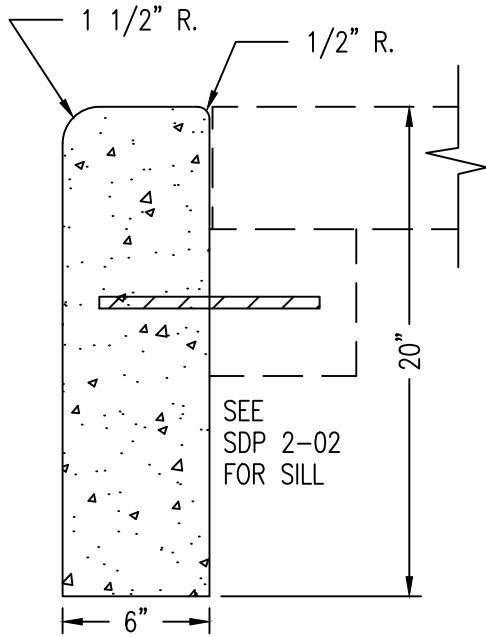
- ①. TIE SILL AT 3'-0" CENTERS WITH 9" x NO. 4 REINFORCING BARS OR POUR INTEGRALLY WITH CURB.
2. SILL SHALL BE INSTALLED ON ALL SIDEWALK LESS THAN 6" THICK, ABUTTING CURB & GUTTER.
- ③. DIMENSION TO BE SAME AS SIDEWALK THICKNESS, 5" MINIMUM.
4. USE AN APPROVED TYPE F 1/4" THICK SEPERATION MATERIAL. MATCH FULL HEIGHT DIMMENSION OF ADJACENT CONCRETE.

DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA

CONCRETE SILL

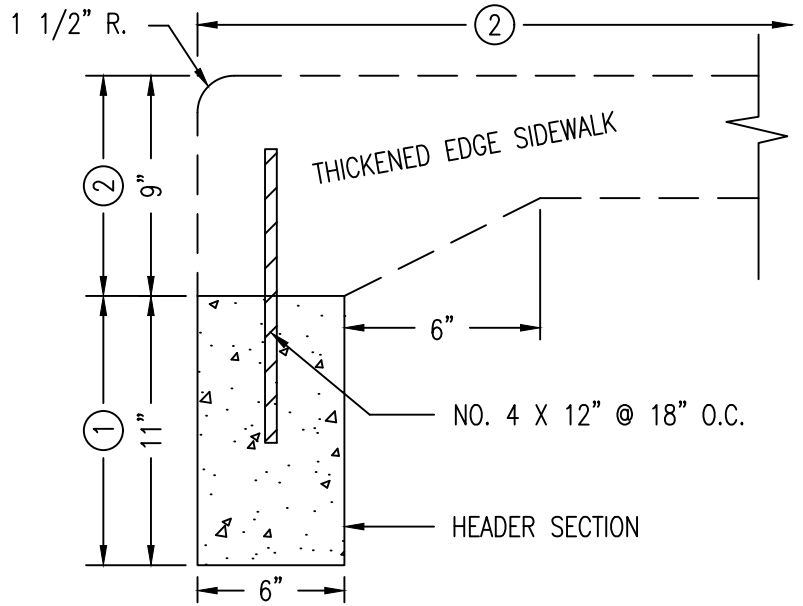
Dylan S. Dahl
CITY ENGINEER

SHT 1 OF 1 SHTS	DATE REVISED 2/2/26	PLATE NO. 2-02	REV. H
-----------------	------------------------	-------------------	-----------

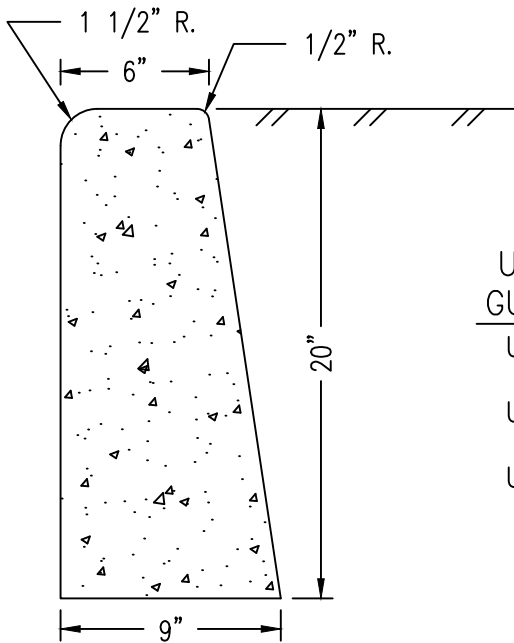


**STRAIGHT CURB
U-1A**

USE WHERE SIDEWALK EXISTS
AND ON CURVES



**CURB & SIDEWALK
U-2A**



**STRAIGHT CURB
U-1B**

USE WHEN THERE IS NO SIDEWALK
AND CURB & GUTTER IS NOT DESIRABLE

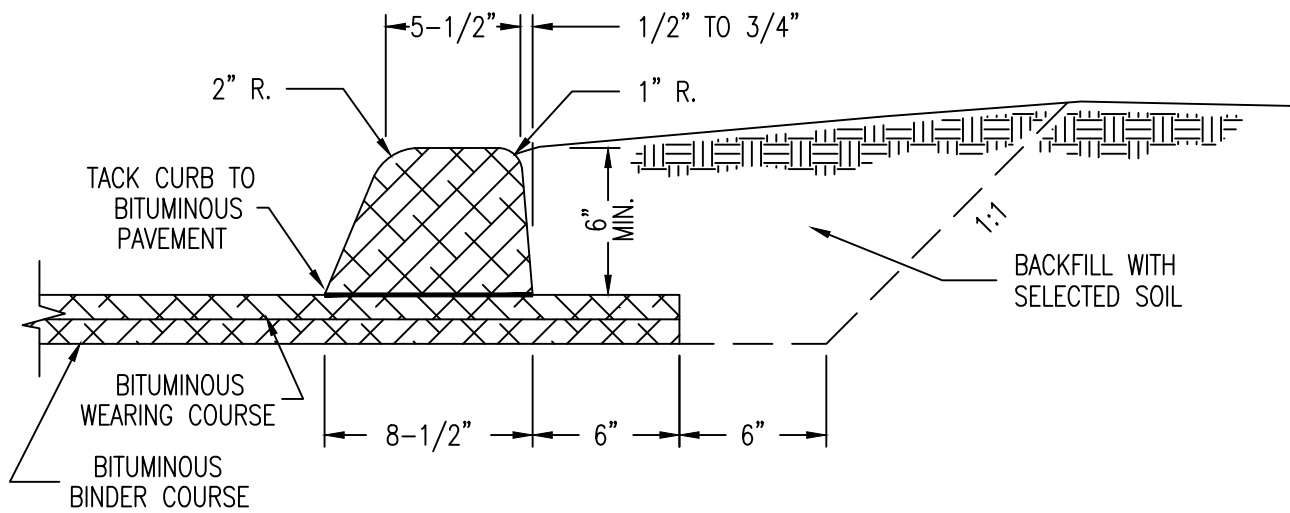
**PAY QUANTITIES
URBAN TYPE CURB,
GUTTER, & SIDEWALK**

U-1A	STRAIGHT CURB LINEAL FEET
U-1B	STRAIGHT CURB LINEAL FEET
U-2A	HEADER SECTION LINEAL FEET SIDEWALK SECTION SQUARE YARD

NOTES

- ① INCLUDED IN PAYMENT FOR U-2A CURB PER LINEAL FOOT.
- ② INCLUDED IN PAYMENT FOR SIDEWALK CONSTRUCTION PER SQUARE FOOT.
3. THESE DETAILS SHALL ONLY BE USED FOR RESTORATION OR PARTIAL RECONSTRUCTION WORK. NEW CONSTRUCTION SHALL USE CURB & GUTTER PER S.D.P. 2-01 OR CURB INTEGRAL TO CONCRETE PAVEMENT.

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA URBAN TYPE CURB AND SIDEWALK			
<i>Douglas L. Nelson</i> ASST. CITY ENGINEER		<i>Reed W. Frenn</i> DIRECTOR	
SHT 1 OF 1 SHTS	DATE REVISED 1/20/17	PLATE NO. 2-03	REV. E

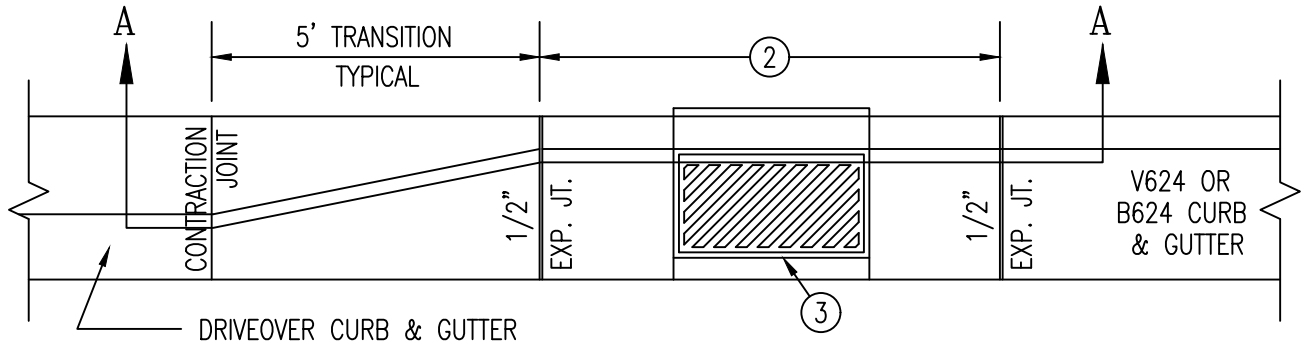


CROSS SECTION

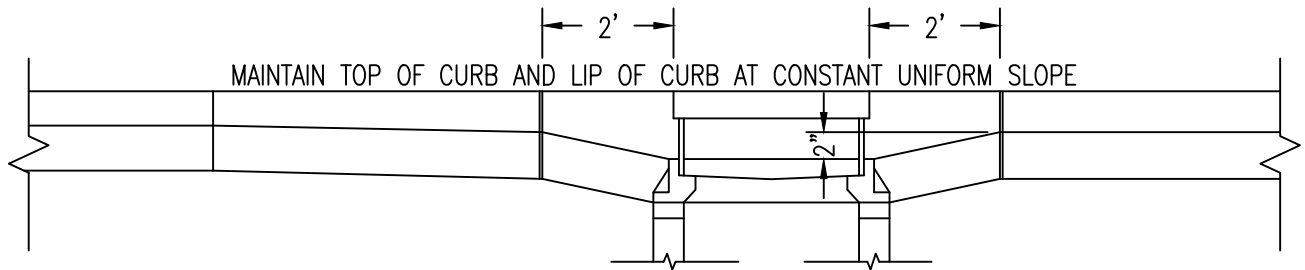
NOTE

VOLUME OF CURB = 0.288 CUBIC FEET PER LINEAL FOOT.
 WEIGHT OF CURB = 21.12 TONS PER 1000 LINEAL FEET.

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA			
BITUMINOUS CURB			
<i>Douglas Nelson</i> ASST. CITY ENGINEER		<i>Reed W. Frenn</i> DIRECTOR	
SHT 1 OF 1 SHTS	DATE REVISED 3/22/06	PLATE NO. 2-04	REV. A



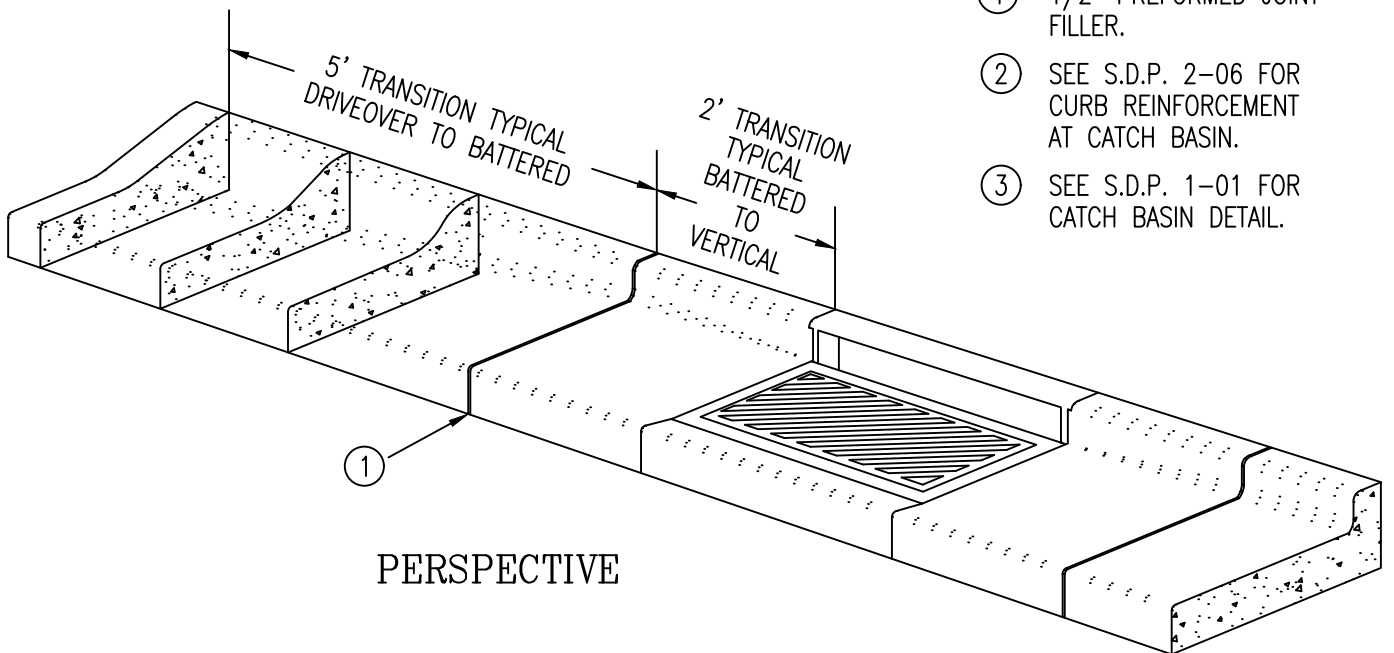
PLAN




SECTION A-A

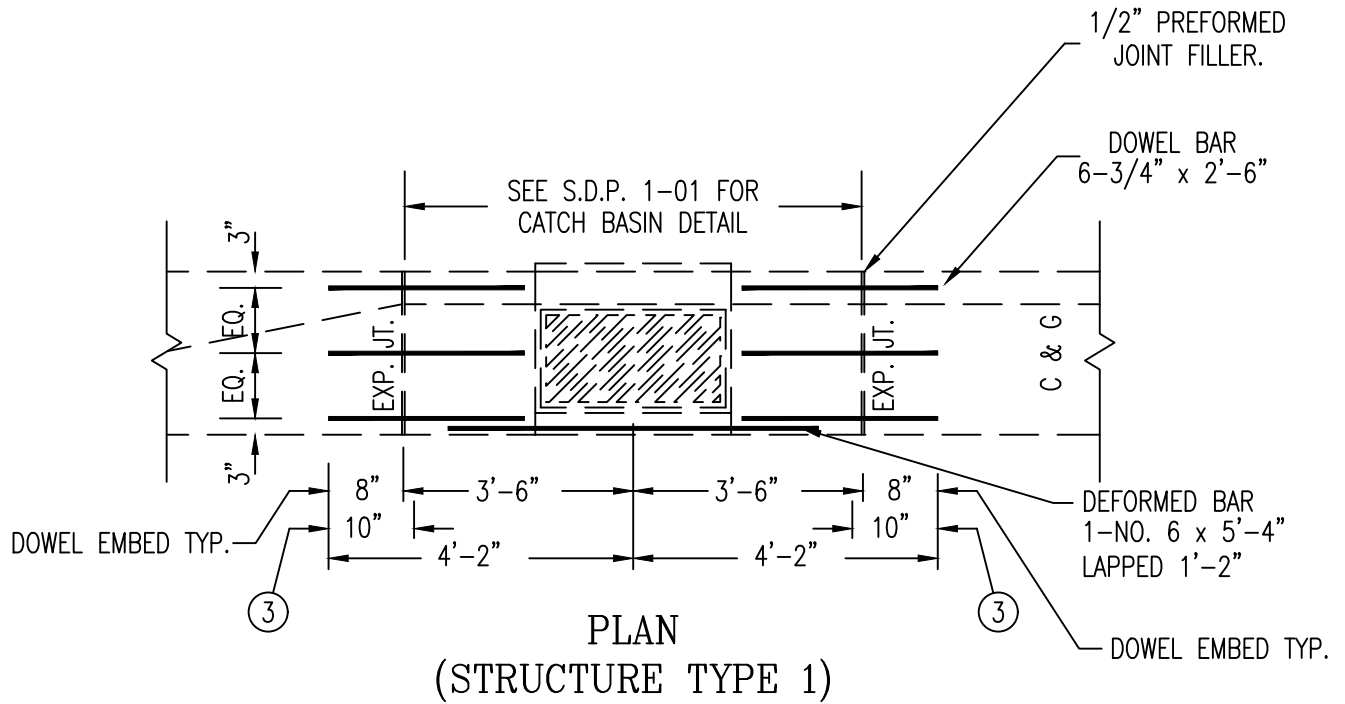
NOTES

- ① 1/2" PREFORMED JOINT FILLER.
- ② SEE S.D.P. 2-06 FOR CURB REINFORCEMENT AT CATCH BASIN.
- ③ SEE S.D.P. 1-01 FOR CATCH BASIN DETAIL.



PERSPECTIVE

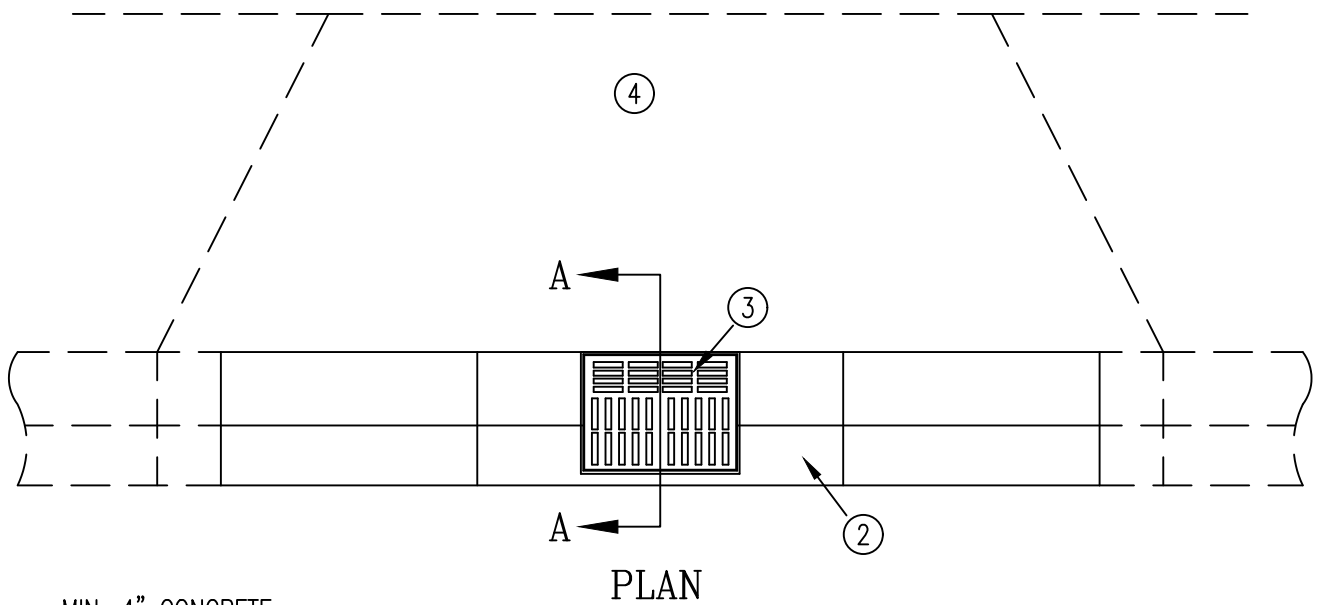
DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA TRANSITION CURB & GUTTER DRIVEOVER TO TYPE B OR V			
 CITY ENGINEER			
SHT 1 OF 1 SHTS	DATE REVISED 1/1/23	PLATE NO. 2-05	REV. D



NOTES

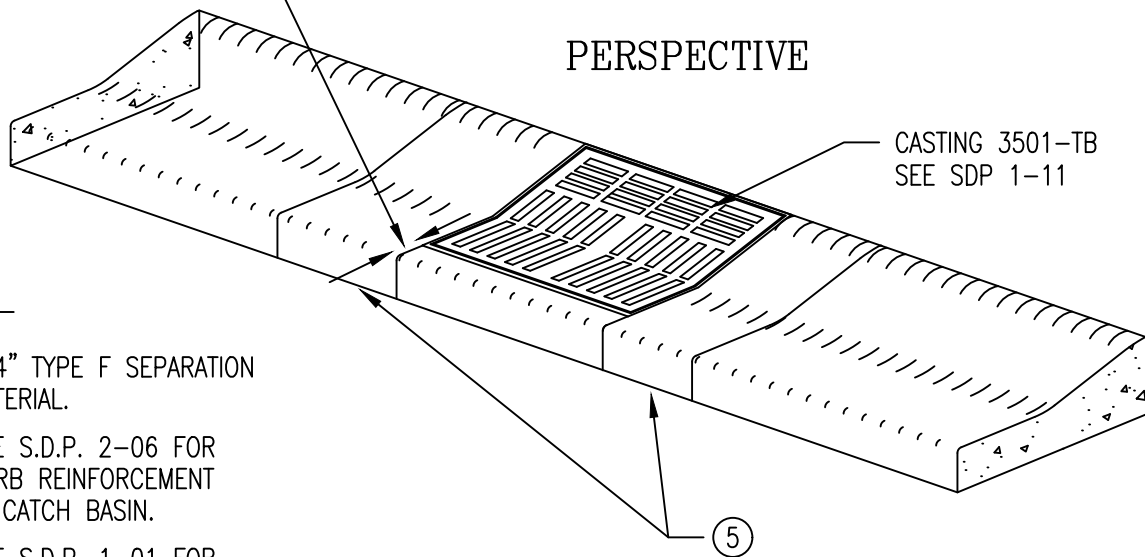
1. DOWEL BARS SHALL BE PER MN/DOT SPEC. 3302 (EPOXY COATED).
2. DEFORMED BAR SHALL BE PER MN/DOT SPEC. 3301 (EPOXY COATED).
- ③ COAT THE DOWEL BARS WITH A THIN UNIFORM COATING OF AN APPROVED FORM COATING MATERIAL MEETING MN/DOT SPEC. 3902 NOT MORE THAN THAN ONE HOUR BEFORE COVERING WITH CONCRETE, OR WRAP WITH TEFLON TAPE.

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA CURB & GUTTER REINFORCEMENT AT CATCH BASINS			
<i>Douglas L. Nelson</i> ASST. CITY ENGINEER		<i>Reed W. Fries</i> DIRECTOR	
SHT 1 OF 1 SHTS	DATE REVISED 1/20/17	PLATE NO. 2-06	REV. E



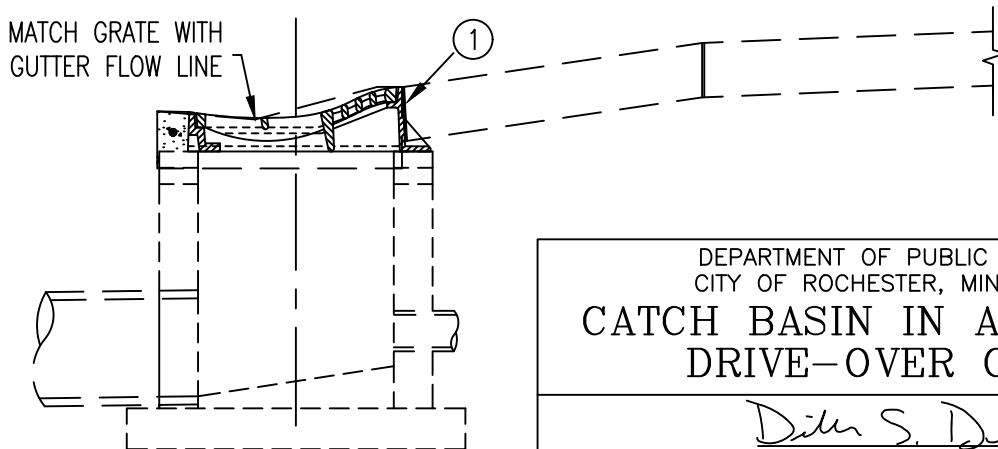
MIN. 4" CONCRETE
TO CURB LIP

PERSPECTIVE

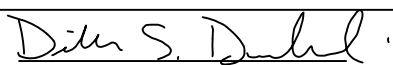


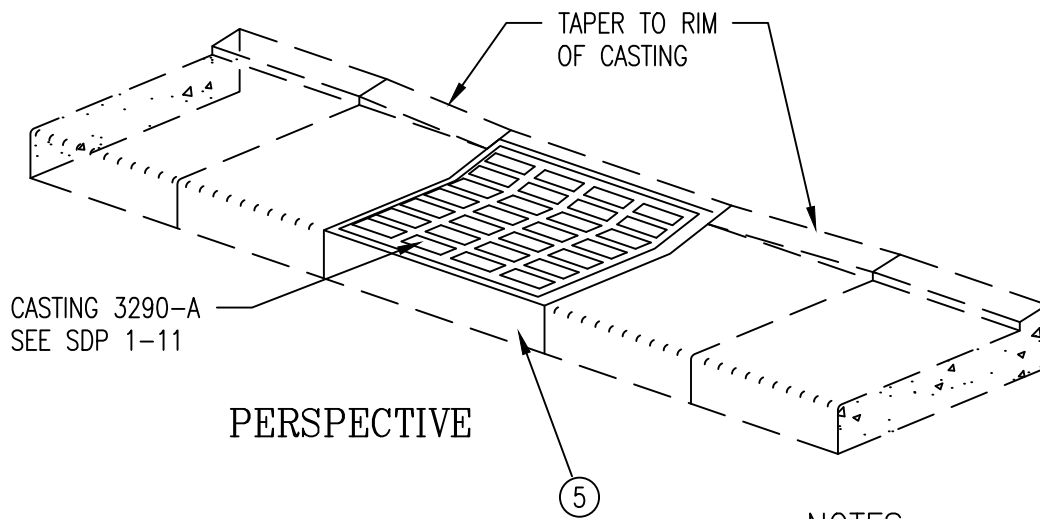
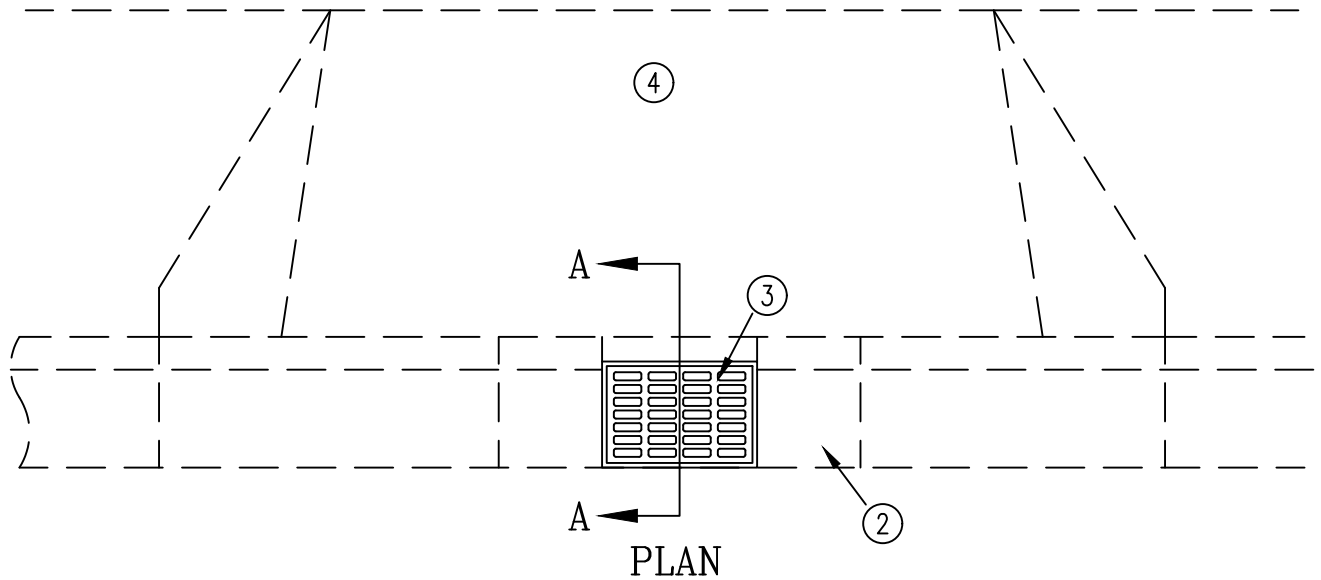
NOTES

- ① 1/4" TYPE F SEPARATION MATERIAL.
- ② SEE S.D.P. 2-06 FOR CURB REINFORCEMENT AT CATCH BASIN.
- ③ SEE S.D.P. 1-01 FOR CATCH BASIN DETAIL.
- ④ SEE S.D.P. 2-08 FOR DRIVE APPROACH DETAIL.
- ⑤ MAX. 10:1 TAPER TO EXPANSION JOINT.



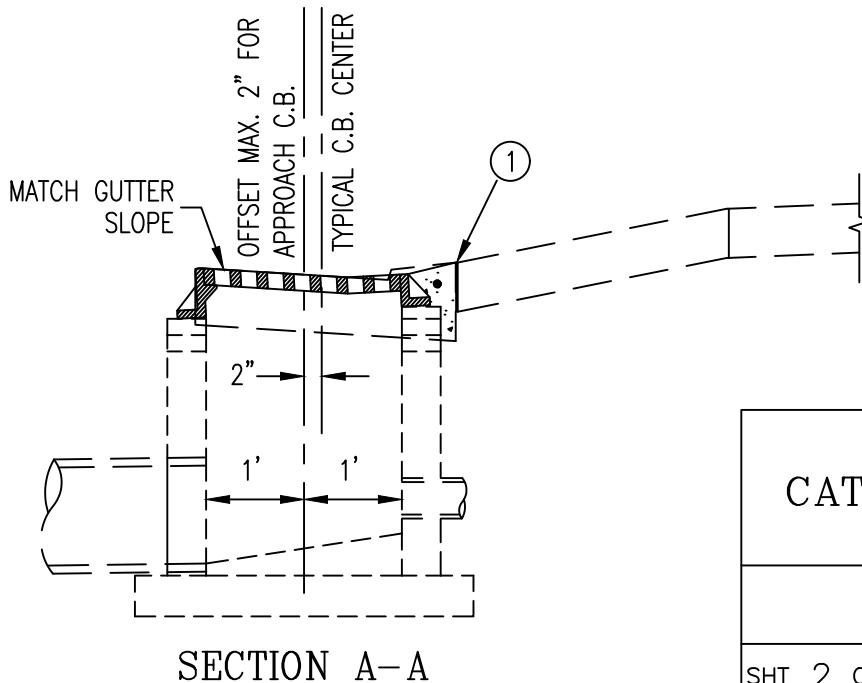
SECTION A-A

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA CATCH BASIN IN APPROACH DRIVE-OVER CURB			
 CITY ENGINEER			
SHT 1 OF 2 SHTS	DATE REVISED 2/2/26	PLATE NO. 2-07	REV. F

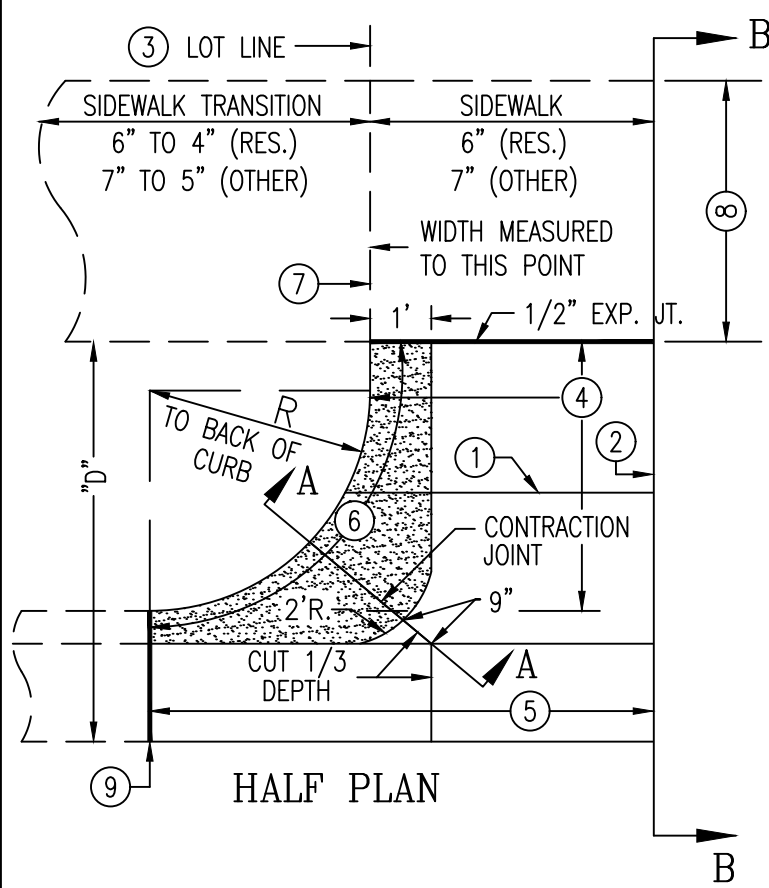


NOTES

- ① 1/4" TYPE F SEPARATION MATERIAL.
- ② SEE S.D.P. 2-06 FOR CURB REINFORCEMENT AT CATCH BASIN.
- ③ SEE S.D.P. 1-01 FOR CATCH BASIN DETAIL.
- ④ SEE S.D.P. 2-08 FOR DRIVE APPROACH DETAIL.
- ⑤ 4" MIN. CONCRETE INFRONT OR BEHIND CASTING.

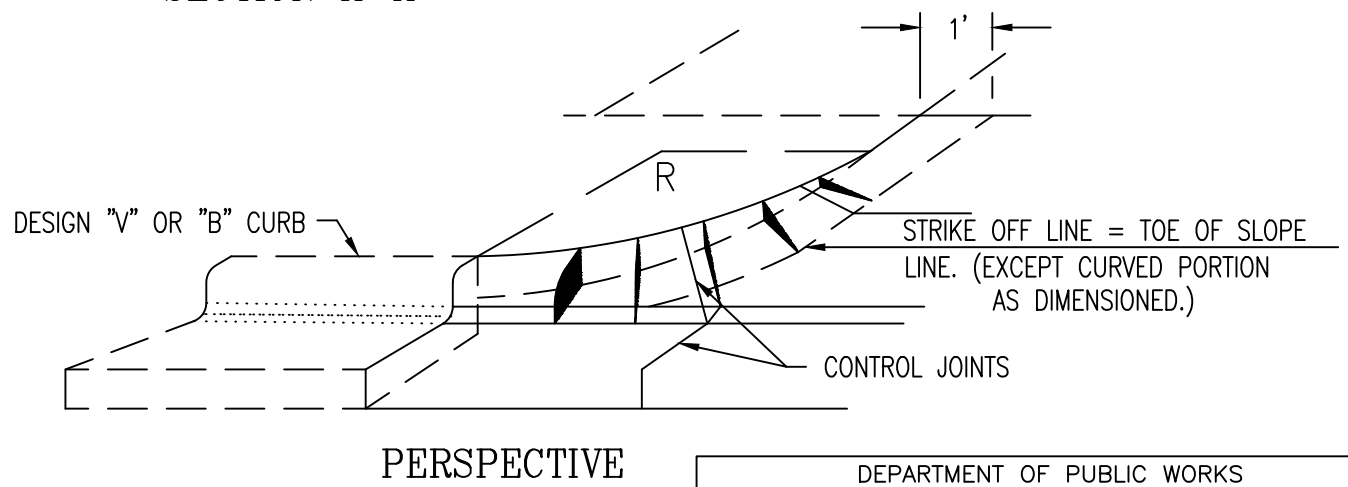
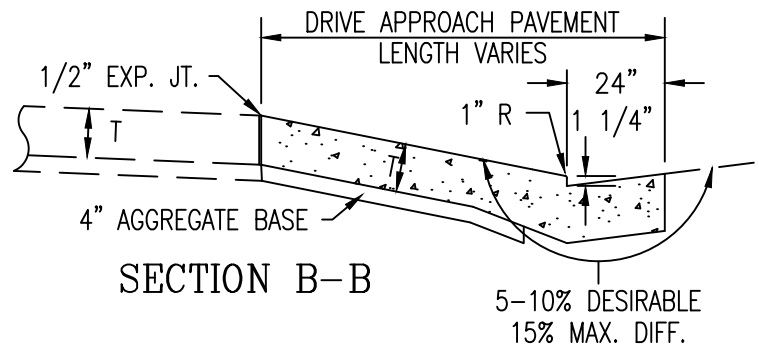
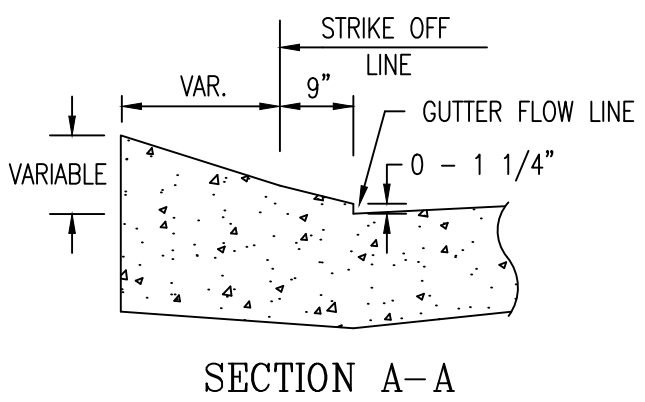


DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA CATCH BASIN IN APPROACH BATTERFACE CURB			
<i>Dimitri S. Dahl</i> CITY ENGINEER			
SHT 2 OF 2 SHTS	DATE REVISED 2/2/26	PLATE NO. 2-07	REV. F



TYPE OF ENTRANCE	WIDTH	R	T
RESIDENTIAL	12'-32'	4'-6"	6"
COMM., IND., ALLEY, R-4	12'-25'	6'-6"	7"

- NOTES**
- ① TRANSVERSE CONTRACTION JOINT REQUIRED WHEN "D" = 10'-0" OR GREATER, PLACE AT THE CENTER OF THE DRIVE APPROACH.
 - ② CONTRACTION JOINTS SPACED SO THAT MAXIMUM SPACING BETWEEN JOINTS DOES NOT EXCEED 1.5 "D" OR 12' WHICHEVER IS LESS, MEASURED AT FLOW LINE.
 - ③ EDGE OF DRIVE APPROACH AT SIDEWALK LINE MUST BE WITHIN THE LOT SERVED.
 - ④ PAID FOR AS CONCRETE DRIVEWAY PAVEMENT.
 - ⑤ PAID FOR AS CURB & GUTTER THRU ENTRANCE.
 - ⑥ SHADED AREA INDICATES CURB TRANSITION FROM FULL EXPOSURE TO ZERO EXPOSURE.
 - ⑦ CONTRACTION JOINT FOR RESIDENTIAL, 1/2" PREFORMED JOINT FILLER.
 - ⑧ MATCH INCOMING PAR WIDTH. IN VERTICALLY CONSTRAINED AREAS, PAR WIDTHS CAN BE INCREMENTALLY REDUCED TO 4' MIN AFTER ALL OTHER OPTIONS HAVE BEEN APPLIED.
 - ⑨ 1/2" PREFORMED JOINT FILLER.



DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA

**CONCRETE DRIVE APPROACH
TYPE A**

Dylan S. Dahl
CITY ENGINEER

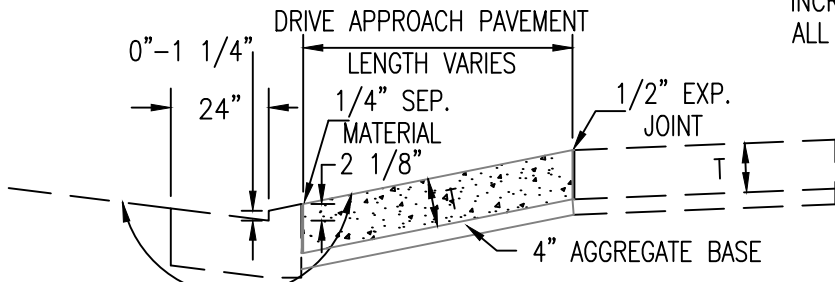
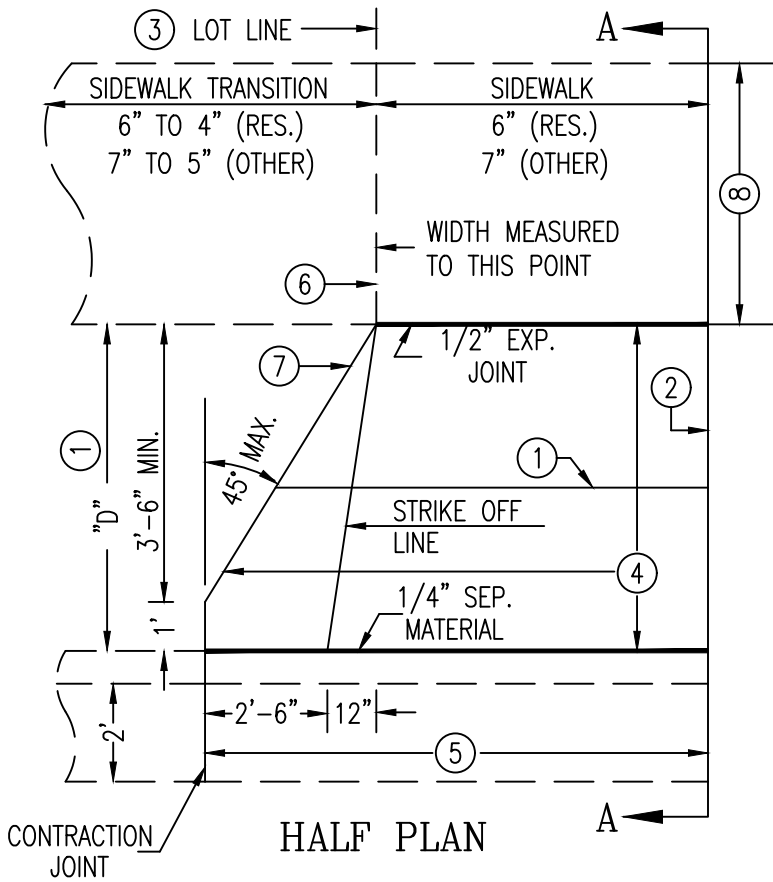
SHT 1 OF 5 SHTS	DATE REVISED 1/1/25	PLATE NO. 2-08	REV. 1
-----------------	------------------------	-------------------	-----------

TYPE OF ENTRANCE	WIDTH	T
RESIDENTIAL, R1, R2	12'-32'	6"
COMM., IND., ALLEY, R-4	12'-25'	7"

T=THICKNESS OF ENTRANCE AND SIDEWALK

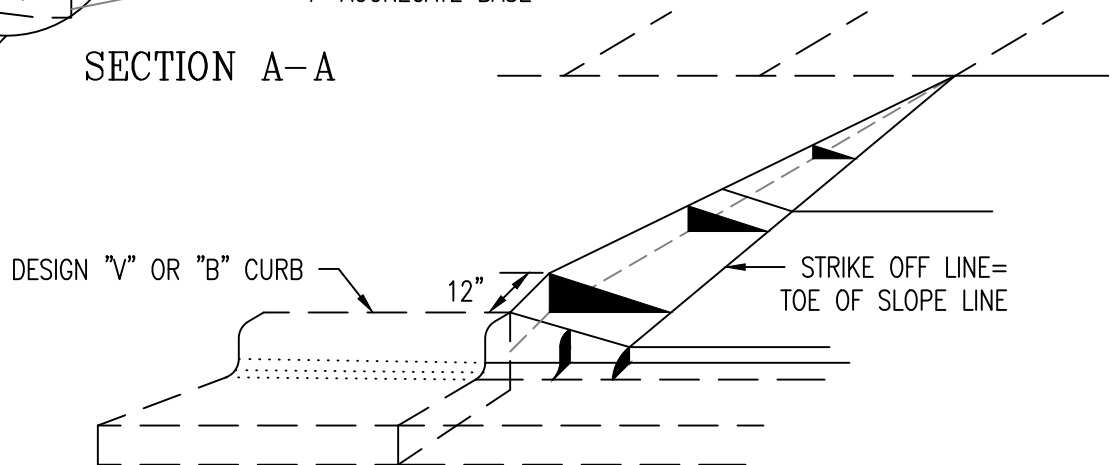
NOTES

- ① TRANSVERSE CONTRACTION JOINT REQUIRED WHEN "D"=10'-0" OR GREATER, PLACED AT THE CENTER OF THE DRIVE APPROACH.
- ② CONTRACTION JOINTS SPACED SO THAT MAXIMUM SPACING BETWEEN JOINTS DOES NOT EXCEED 1.5 "D" OR 12' WHICHEVER IS LESS, MEASURED AT THE BACK OF CURB.
- ③ EDGE OF DRIVE APPROACH AT SIDEWALK LINE MUST BE WITHIN THE LOT SERVED.
- ④ PAID FOR AS CONCRETE DRIVEWAY PAVEMENT.
- ⑤ NO DEDUCTION TO BE MADE IN CURB & GUTTER FOR ENTRANCE.
- ⑥ CONTRACTION JOINT FOR RESIDENTIAL, 1/2" PREFORMED JOINT FILLER FOR OTHER.
- ⑦ TAPER WING TO SIDEWALK WHEN "D" IS 15' OR LESS, OTHERWISE TAPER TO 1/2 "D".
- ⑧ MATCH INCOMING PAR WIDTH. IN VERTICALLY CONSTRAINED AREAS, PAR WIDTHS CAN BE INCREMENTALLY REDUCED TO 4' MIN AFTER ALL OTHER OPTIONS HAVE BEEN APPLIED.

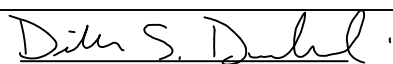


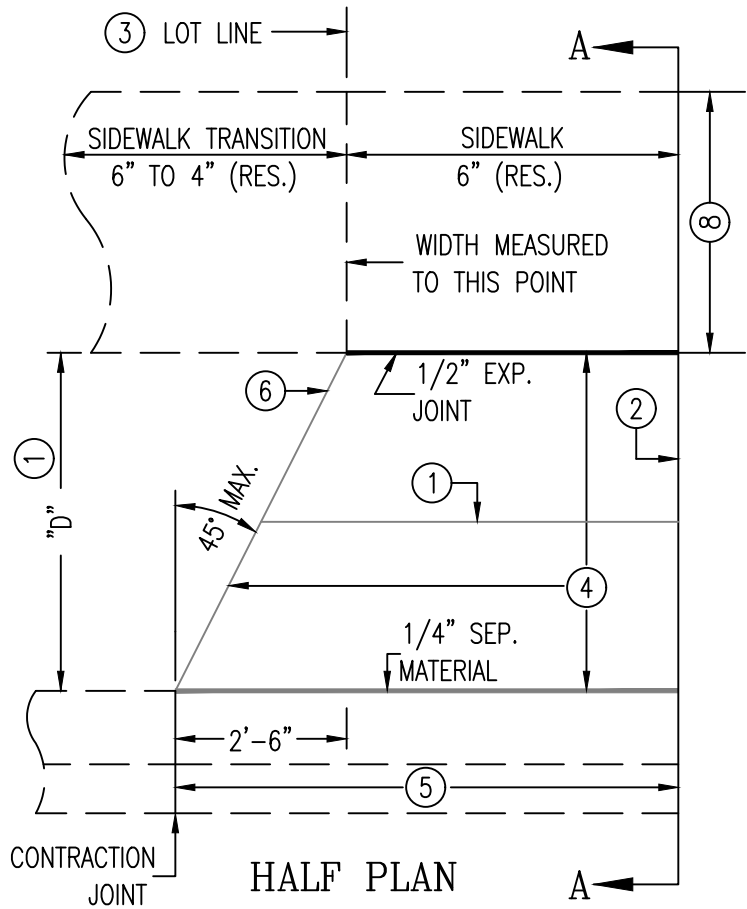
5-10% DESIRABLE
15% MAX. DIFF.

SECTION A-A



PERSPECTIVE

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA CONCRETE DRIVE APPROACH TYPE B			
 CITY ENGINEER			
SHT 2 OF 5 SHTS	DATE REVISED 1/1/25	PLATE NO. 2-08	REV. 1

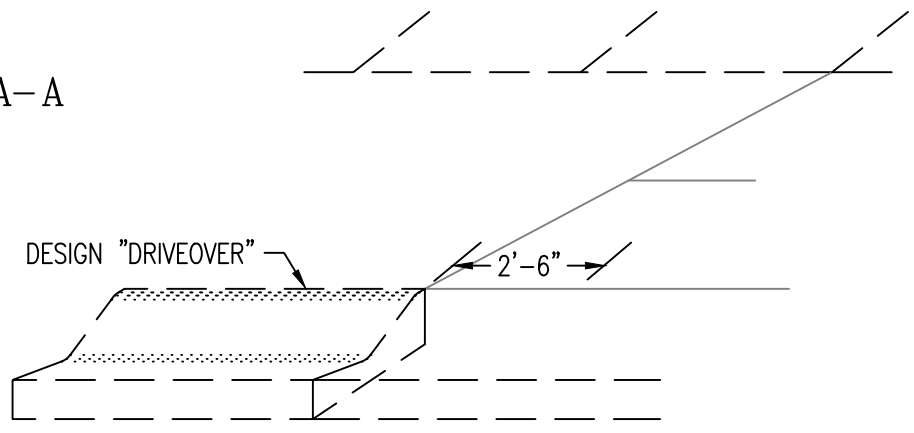
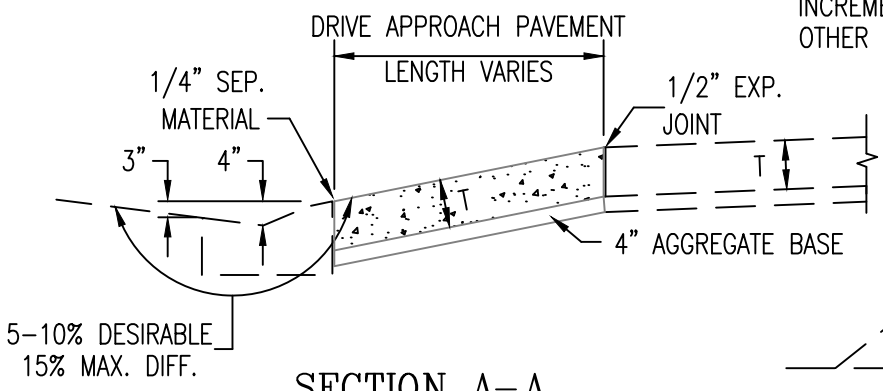


TYPE OF ENTRANCE	WIDTH	T
RESIDENTIAL	12'-32'	6"

T=THICKNESS OF ENTRANCE AND SIDEWALK

NOTES

- ① TRANSVERSE CONTRACTION JOINT REQUIRED WHEN "D"=10'-0" OR GREATER, PLACED AT THE CENTER OF THE DRIVE APPROACH.
- ② CONTRACTION JOINTS SPACED SO THAT MAXIMUM SPACING BETWEEN JOINTS DOES NOT EXCEED 1.5 "D" OR 12' WHICHEVER IS LESS, MEASURED AT THE BACK OF CURB.
- ③ EDGE OF DRIVE APPROACH AT SIDEWALK LINE MUST BE WITHIN THE LOT SERVED.
- ④ PAID FOR AS CONCRETE DRIVEWAY PAVEMENT.
- ⑤ NO DEDUCTION TO BE MADE IN CURB & GUTTER FOR ENTRANCE.
- ⑥ TAPER WING TO SIDEWALK WHEN "D" IS 15' OR LESS, OTHERWISE TAPER TO 1/2 "D".
7. EXPANSION MATERIAL TO BE PREFORMED JOINT FILLER. SEPARATION MATERIAL TO BE TYPE F.
- ⑧ MATCH INCOMING PAR WIDTH. IN VERTICALLY CONSTRAINED AREAS, PAR WIDTHS CAN BE INCREMENTALLY REDUCED TO 4' MIN AFTER ALL OTHER OPTIONS HAVE BEEN APPLIED.



DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA

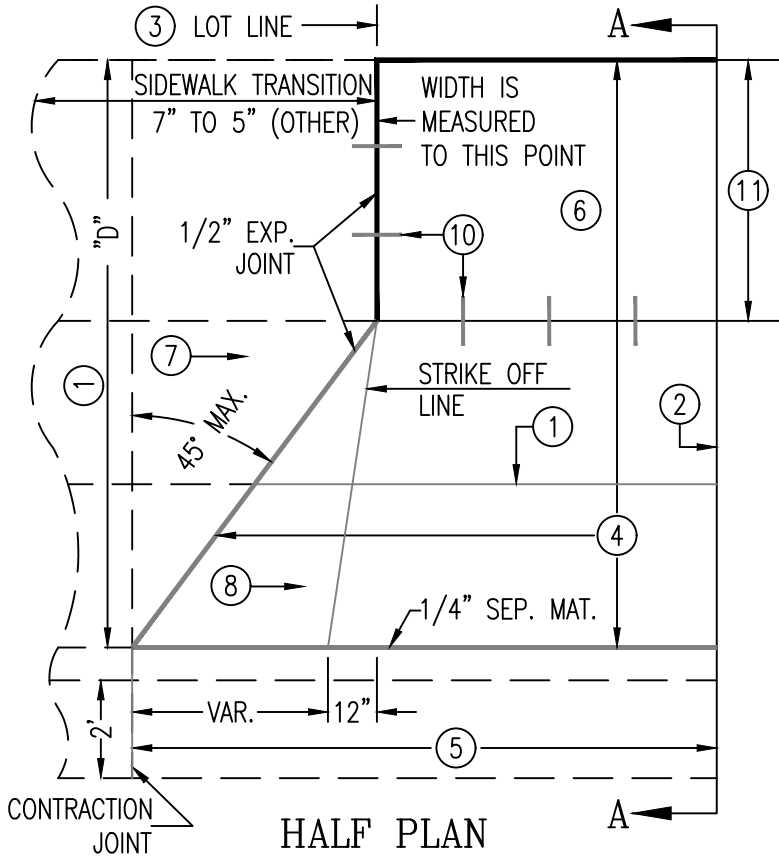
**CONCRETE DRIVE APPROACH
TYPE C**

Dimitri S. Danil
CITY ENGINEER

SHT 3 OF 5 SHTS	DATE REVISED 1/1/25	PLATE NO. 2-08	REV. 1
-----------------	------------------------	-------------------	-----------

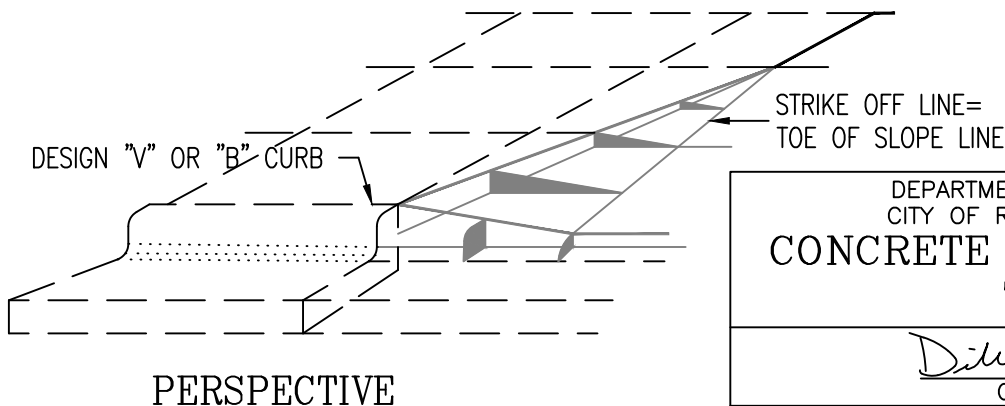
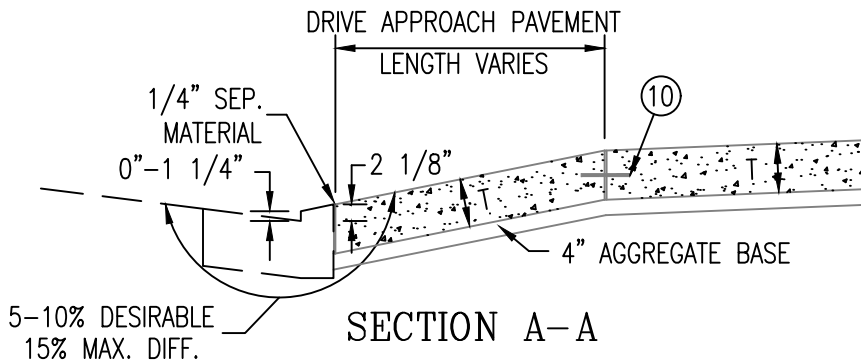
TYPE OF ENTRANCE	WIDTH	T
COMM., IND., ALLEY, R-4	12'-25'	7"

T=THICKNESS OF ENTRANCE AND SIDEWALK



NOTES

- ① TRANSVERSE CONTRACTION JOINT REQUIRED WHEN "D"=10'-0" OR GREATER, PLACED AT THE CENTER OF THE DRIVE APPROACH.
- ② CONTRACTION JOINTS SPACED SO THAT MAXIMUM SPACING BETWEEN JOINTS DOES NOT EXCEED 1.5 "D" OR 12' WHICHEVER IS LESS, MEASURED AT THE BACK OF CURB.
- ③ EDGE OF DRIVE APPROACH AT SIDEWALK LINE MUST BE WITHIN THE LOT SERVED.
- ④ PAID FOR AS CONCRETE DRIVEWAY PAVEMENT.
- ⑤ NO DEDUCTION TO BE MADE IN CURB & GUTTER FOR ENTRANCE.
- ⑥ CROSS SLOPE ON SIDEWALK IS TO BE 2%
- ⑦ SLOPE ADJACENT SIDEWALK AS NECESSARY, TO A MAXIMUM PITCH OF 1:20.
- ⑧ WING SLOPE IS TO HAVE A MAXIMUM PITCH OF 1:10.
9. EXPANSION MATERIAL TO BE PREFORMED JOINT FILLER. SEPARATION MATERIAL TO BE TYPE F.
- ⑩ #4 x 12" LONG TIE BARS (EPOXY COATED) 36" MAX. SPACING
- ⑪ MATCH INCOMING PAR WIDTH. IN VERTICALLY CONSTRAINED AREAS, PAR WIDTHS CAN BE INCREMENTALLY REDUCED TO 4' MIN AFTER ALL OTHER OPTIONS HAVE BEEN APPLIED.



DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA
**CONCRETE DRIVE APPROACH
TYPE D**

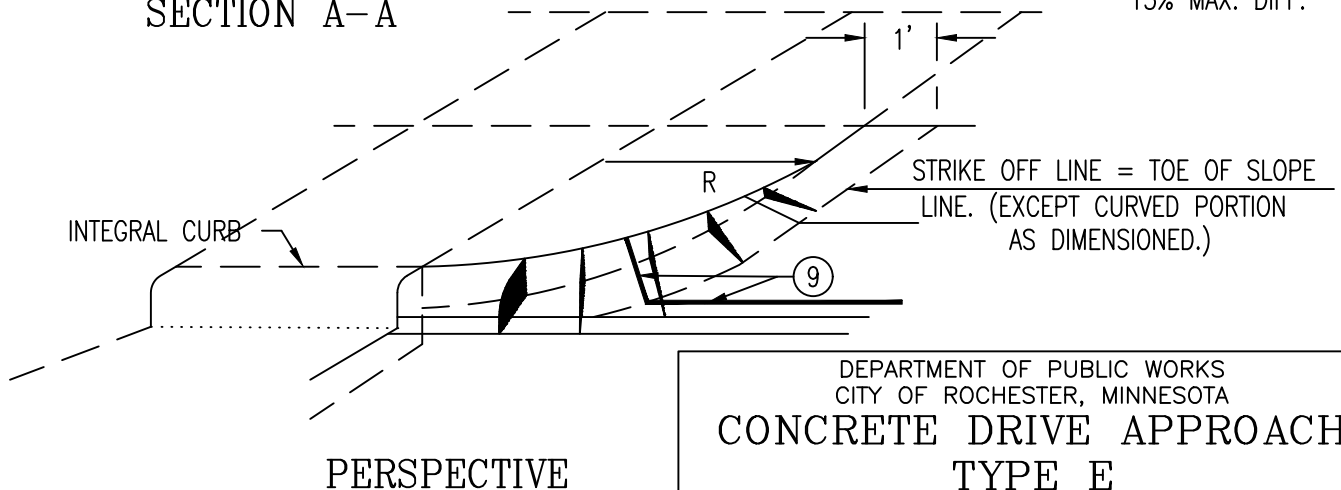
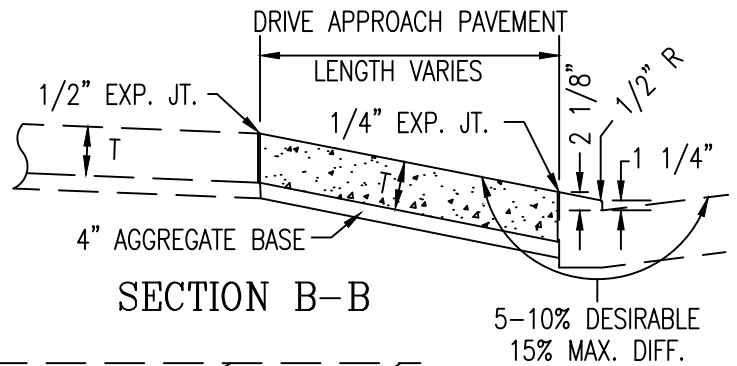
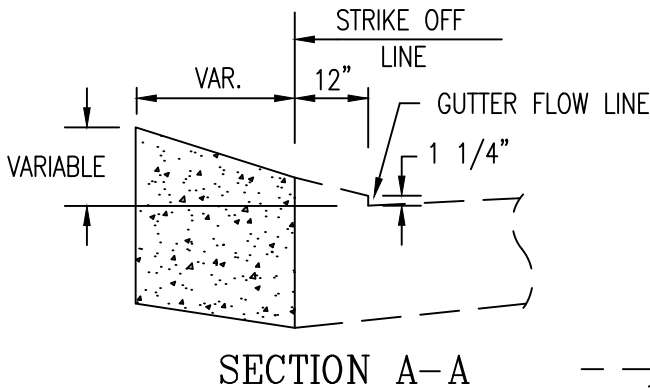
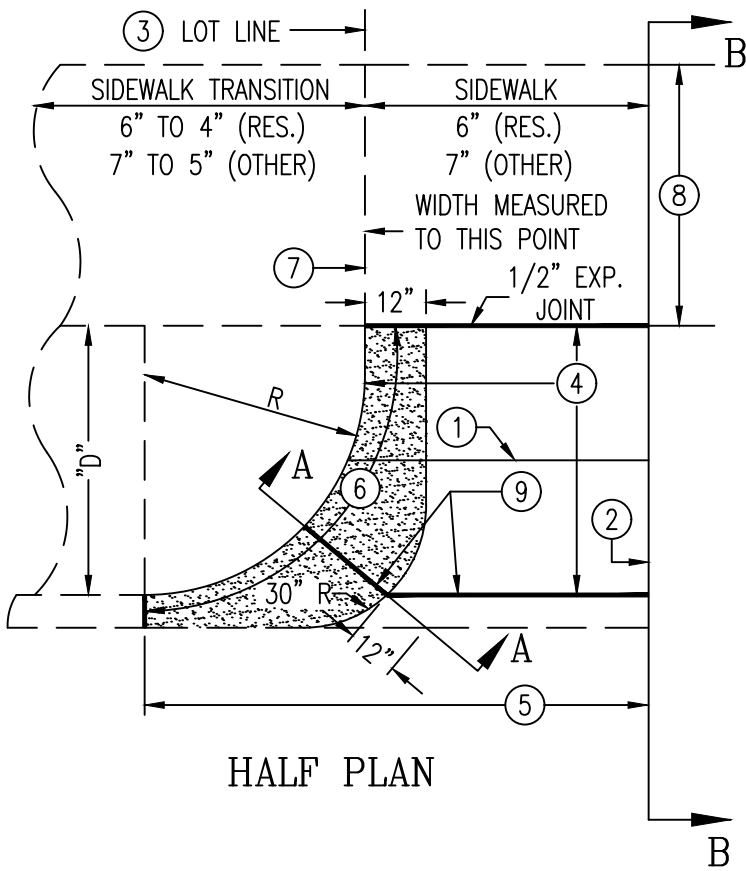
Dimitri S. Dandil
CITY ENGINEER

SHT 4 OF 5 SHTS	DATE REVISED 1/1/25	PLATE NO. 2-08	REV. 1
-----------------	------------------------	-------------------	-----------

TYPE OF ENTRANCE	WIDTH	R	T
RESIDENTIAL	12'-32'	4'-6"	6"
COMM., IND., ALLEY, R-4	12'-25'	6'-6"	7"

NOTES

- ① TRANSVERSE CONTRACTION JOINT REQUIRED WHEN "D" = 10'-0" OR GREATER, PLACED AT THE CENTER OF THE DRIVE APPROACH.
- ② CONTRACTION JOINTS SPACED SO THAT MAXIMUM SPACING BETWEEN JOINTS DOES NOT EXCEED 1.5 "D" OR 12' WHICHEVER IS LESS, MEASURED AT FLOW LINE.
- ③ EDGE OF DRIVE APPROACH AT SIDEWALK LINE MUST BE WITHIN THE LOT SERVED.
- ④ PAID FOR AS CONCRETE DRIVEWAY PAVEMENT.
- ⑤ NO DEDUCTION TO BE MADE IN INTEGRANT CURB AT ENTRANCE.
- ⑥ SHADED AREA INDICATES CURB TRANSITION FROM FULL EXPOSURE TO ZERO EXPOSURE.
- ⑦ CONTRACTION JOINT FOR RESIDENTIAL, 1/2" PREFORMED JOINT FILLER.
- ⑧ MATCH INCOMING PAR WIDTH. IN VERTICALLY CONSTRAINED AREAS, PAR WIDTHS CAN BE INCREMENTALLY REDUCED TO 4' MIN AFTER ALL OTHER OPTIONS HAVE BEEN APPLIED.
- ⑨ 1/4" TYPE F SEPARATION MATERIAL.

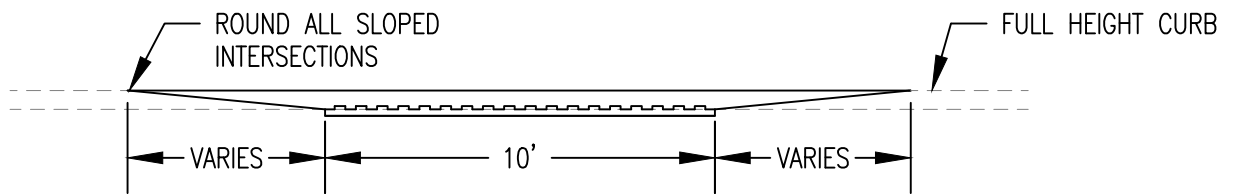
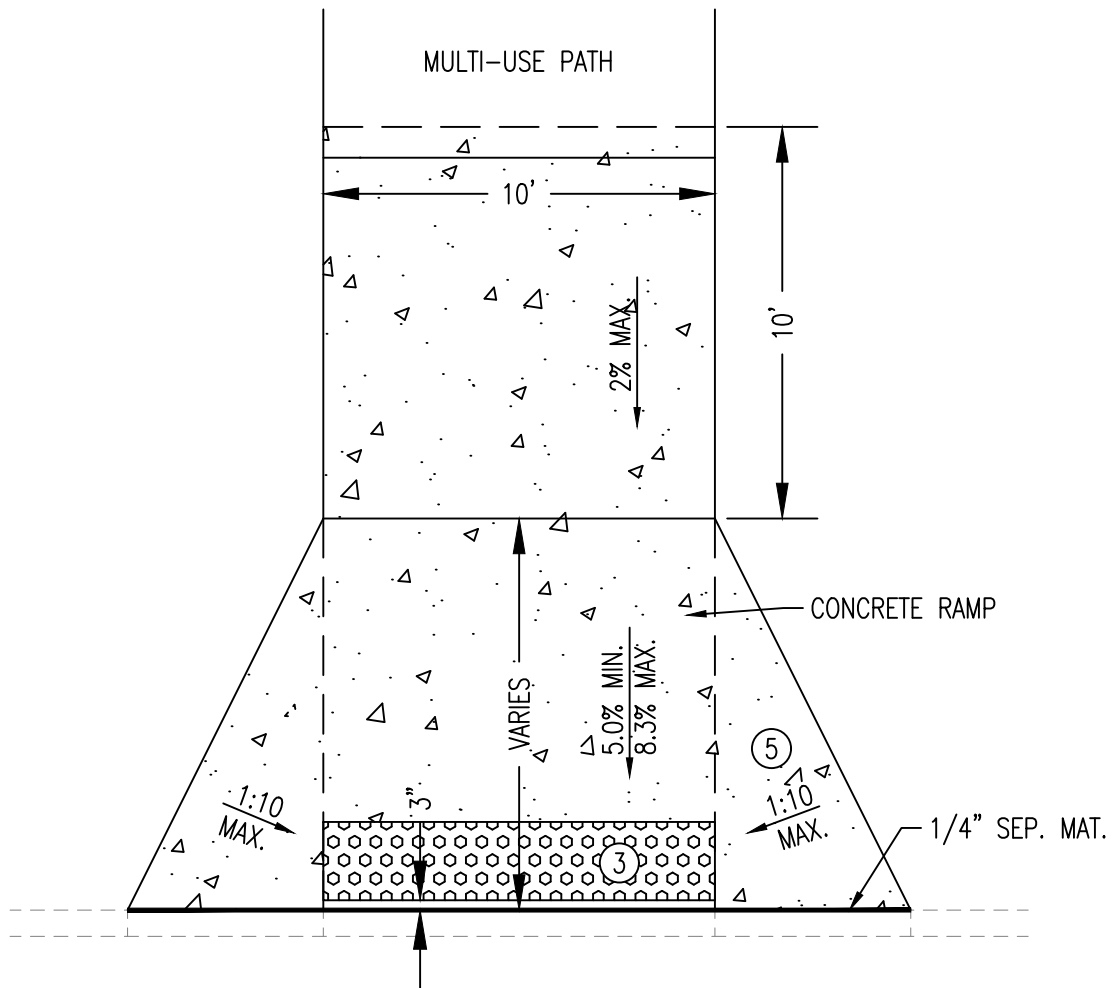


DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA

CONCRETE DRIVE APPROACH TYPE E

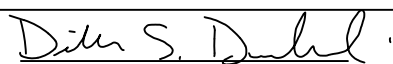
Dimitri S. Danil
CITY ENGINEER

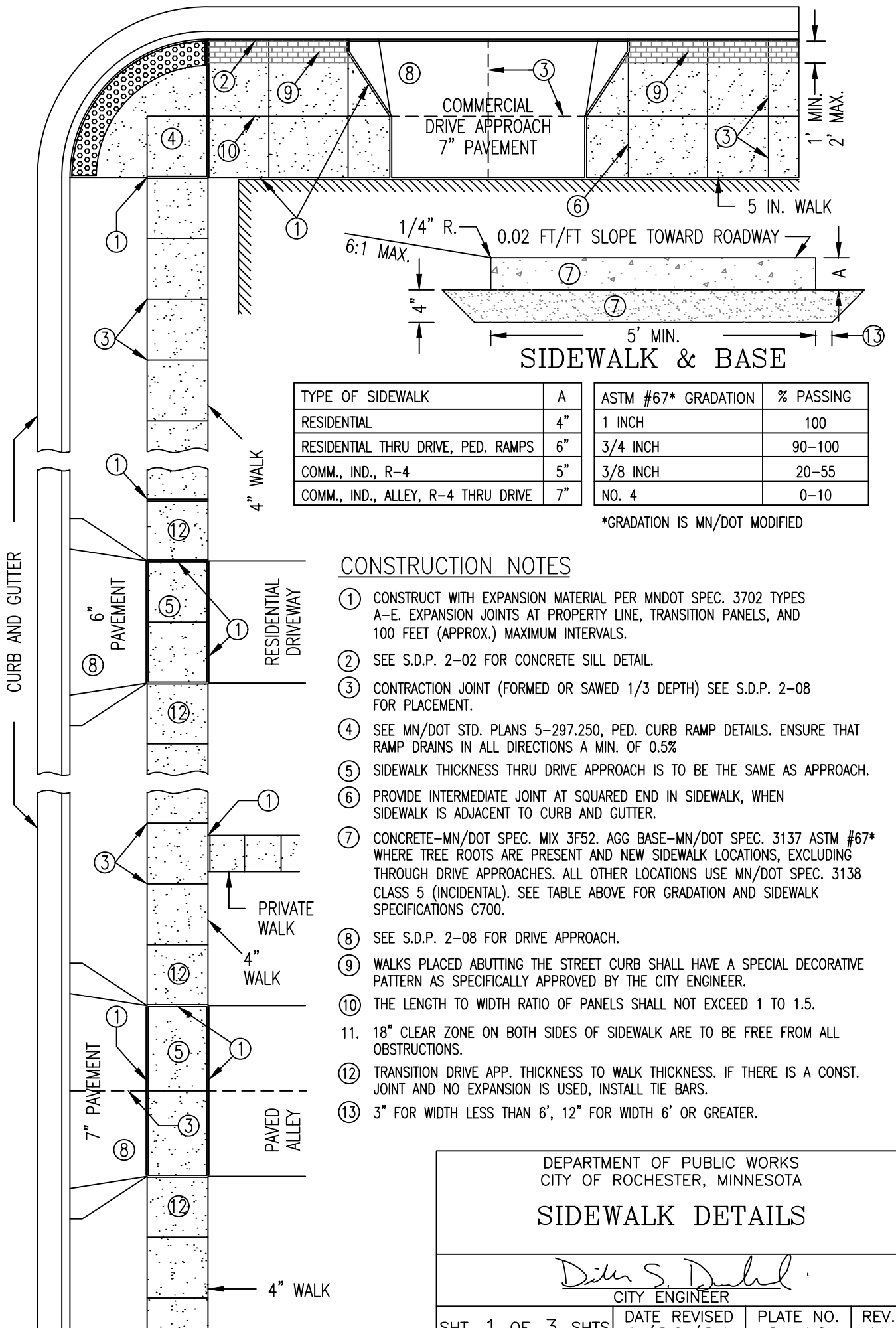
SHT 5 OF 5 SHTS	DATE REVISED 1/1/25	PLATE NO. 2-08	REV. 1
-----------------	------------------------	-------------------	-----------



NOTES

1. DETAIL COMPLIES WITH THE AMERICANS WITH DISABILITIES ACT (ADA) REQUIREMENTS.
2. SEE MN/DOT STANDARD PLAN 5-297.250 PEDESTRIAN CURB RAMP DETAILS
- ③ IF RAMP IS PAID BY SQ. FT. THEN DETECTABLE WARNING AREA IS PAID IN ADDITION TO RAMP AREA.
4. RAMP THICKNESS IS TO BE MINIMUM 6" CONCRETE-MN/DOT SPEC. MIX 3F52 WITH A MINIMUM OF 4" AGGREGATE BASE (INCIDENTAL).
- ⑤ CONSTRUCT WINGS IN WALKABLE SURFACE AREAS.

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA			
MULTI-USE PATH CURB RAMP			
 CITY ENGINEER			
SHT 1 OF 1 SHTS	DATE REVISED 1/26/24	PLATE NO. 2-09	REV. 0



TYPE OF SIDEWALK	A
RESIDENTIAL	4"
RESIDENTIAL THRU DRIVE, PED. RAMPS	6"
COMM., IND., R-4	5"
COMM., IND., ALLEY, R-4 THRU DRIVE	7"

ASTM #67* GRADATION	% PASSING
1 INCH	100
3/4 INCH	90-100
3/8 INCH	20-55
NO. 4	0-10

*GRADATION IS MN/DOT MODIFIED

CONSTRUCTION NOTES

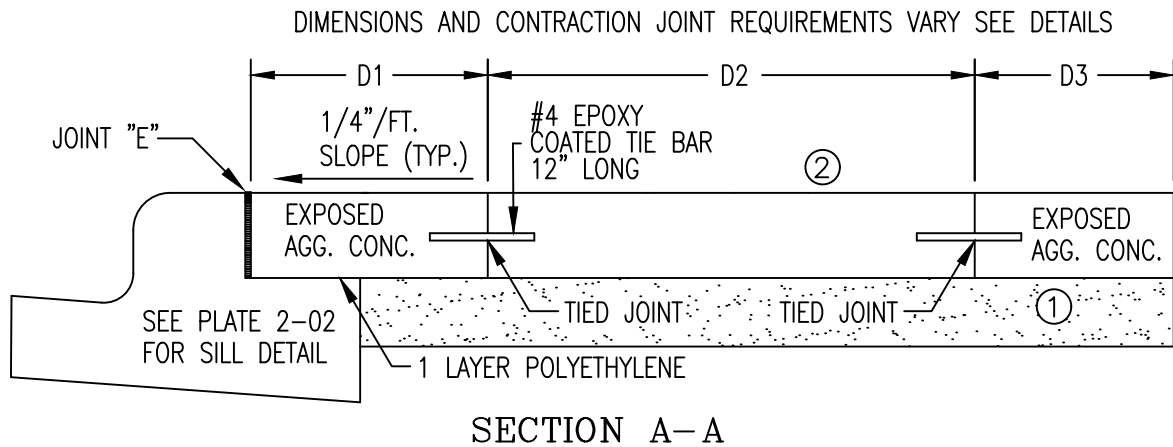
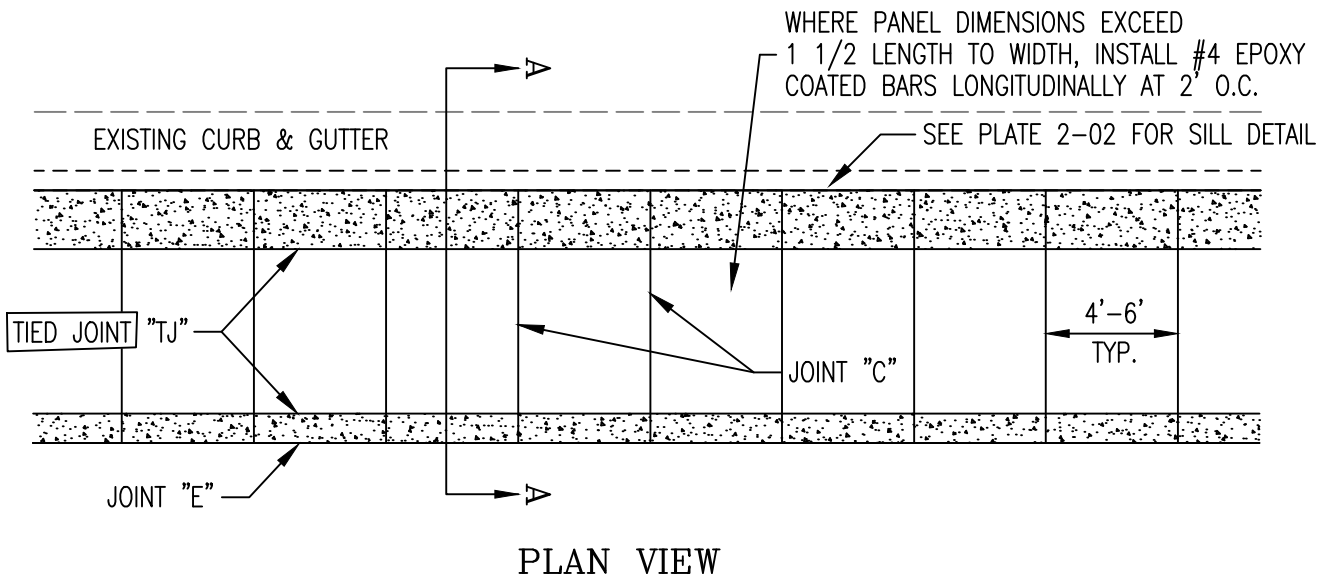
- ① CONSTRUCT WITH EXPANSION MATERIAL PER MNDOT SPEC. 3702 TYPES A-E. EXPANSION JOINTS AT PROPERTY LINE, TRANSITION PANELS, AND 100 FEET (APPROX.) MAXIMUM INTERVALS.
- ② SEE S.D.P. 2-02 FOR CONCRETE SILL DETAIL.
- ③ CONTRACTION JOINT (FORMED OR SAWED 1/3 DEPTH) SEE S.D.P. 2-08 FOR PLACEMENT.
- ④ SEE MN/DOT STD. PLANS 5-297.250, PED. CURB RAMP DETAILS. ENSURE THAT RAMP DRAINS IN ALL DIRECTIONS A MIN. OF 0.5%
- ⑤ SIDEWALK THICKNESS THRU DRIVE APPROACH IS TO BE THE SAME AS APPROACH.
- ⑥ PROVIDE INTERMEDIATE JOINT AT SQUARED END IN SIDEWALK, WHEN SIDEWALK IS ADJACENT TO CURB AND GUTTER.
- ⑦ CONCRETE-MN/DOT SPEC. MIX 3F52. AGG BASE-MN/DOT SPEC. 3137 ASTM #67* WHERE TREE ROOTS ARE PRESENT AND NEW SIDEWALK LOCATIONS, EXCLUDING THROUGH DRIVE APPROACHES. ALL OTHER LOCATIONS USE MN/DOT SPEC. 3138 CLASS 5 (INCIDENTAL). SEE TABLE ABOVE FOR GRADATION AND SIDEWALK SPECIFICATIONS C700.
- ⑧ SEE S.D.P. 2-08 FOR DRIVE APPROACH.
- ⑨ WALKS PLACED ABUTTING THE STREET CURB SHALL HAVE A SPECIAL DECORATIVE PATTERN AS SPECIFICALLY APPROVED BY THE CITY ENGINEER.
- ⑩ THE LENGTH TO WIDTH RATIO OF PANELS SHALL NOT EXCEED 1 TO 1.5.
11. 18" CLEAR ZONE ON BOTH SIDES OF SIDEWALK ARE TO BE FREE FROM ALL OBSTRUCTIONS.
- ⑫ TRANSITION DRIVE APP. THICKNESS TO WALK THICKNESS. IF THERE IS A CONST. JOINT AND NO EXPANSION IS USED, INSTALL TIE BARS.
- ⑬ 3" FOR WIDTH LESS THAN 6', 12" FOR WIDTH 6' OR GREATER.

DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA

SIDEWALK DETAILS

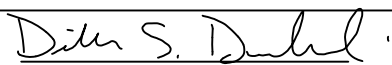
Dylan S. Dahl
CITY ENGINEER

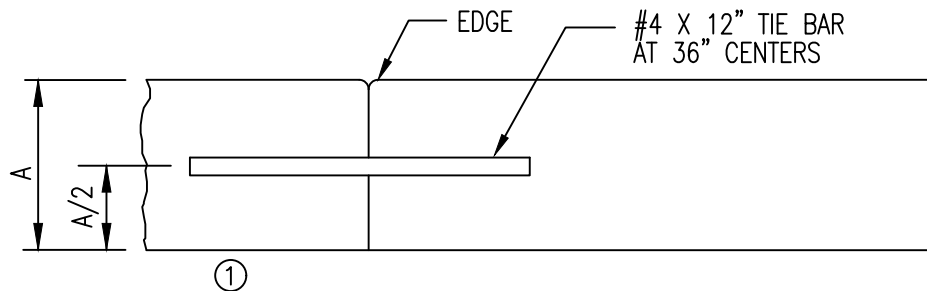
SHT 1 OF 3 SHTS	DATE REVISED 1/26/24	PLATE NO. 2-10	REV. N
-----------------	-------------------------	-------------------	-----------



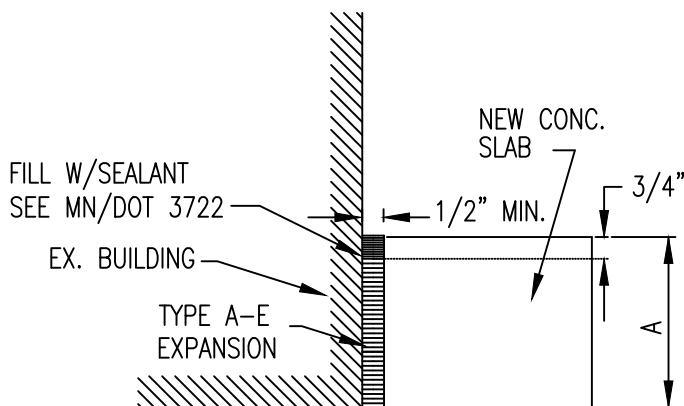
CONSTRUCTION NOTES

- ① PLACE 4" AGGREGATE BASE BELOW SLAB (INCIDENTAL)
- ② CONCRETE SURFACE SHALL BE BROOM FINISHED
3. TIE ALL JOINTS BETWEEN EXPOSED AGGREGATE AND BROOM FINISH CONCRETE, TRANSVERSE AND LONGITUDINAL JOINTS (INCIDENTAL).
4. SEAL JOINTS WITH APPROVED FILLER AND SILICONE JOINT AND CRACK SEALER MEETING MN/DOT SPEC 3722 (INCIDENTAL).

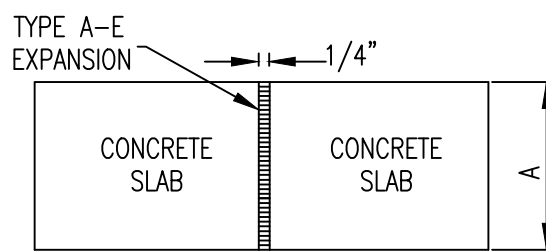
DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA SIDEWALK DETAILS DECORATIVE EDGING			
 CITY ENGINEER			
SHT 2 OF 3 SHTS	DATE REVISED 1/26/24	PLATE NO. 2-10	REV. N



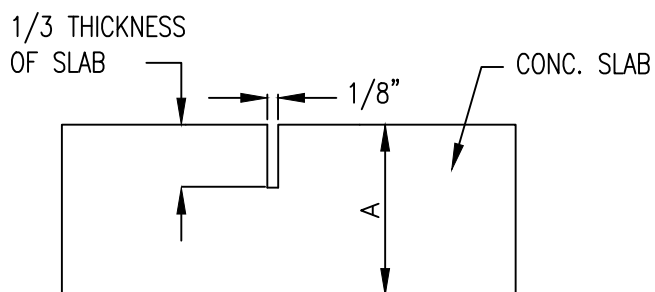
①
TIED JOINT ③
"TJ"



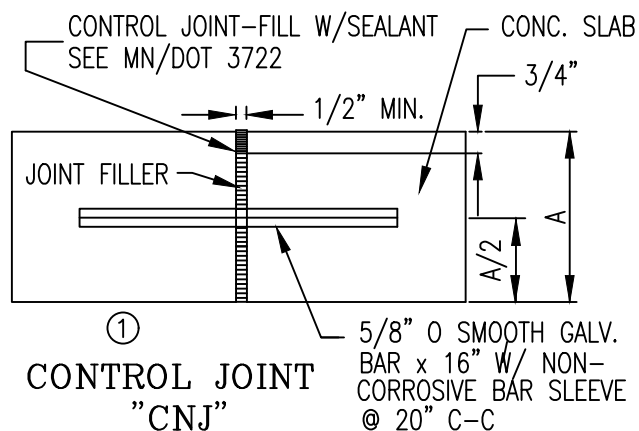
①
FOR USE AROUND FIXED OBJECTS AND BACK OF CURB
EX. BUILDINGS, LIGHTS, HYDRANTS
EXPANSION JOINT
"E"



①
FOR USE IN CONCRETE SLABS
EX. SIDEWALK, DRIVEWAYS
EXPANSION JOINT
"E"



CONTRACTION JOINT
"C"



①
CONTROL JOINT
"CNJ"
5/8" Ø SMOOTH GALV.
BAR x 16" W/ NON-
CORROSIVE BAR SLEEVE
@ 20" C-C

CONSTRUCTION NOTES

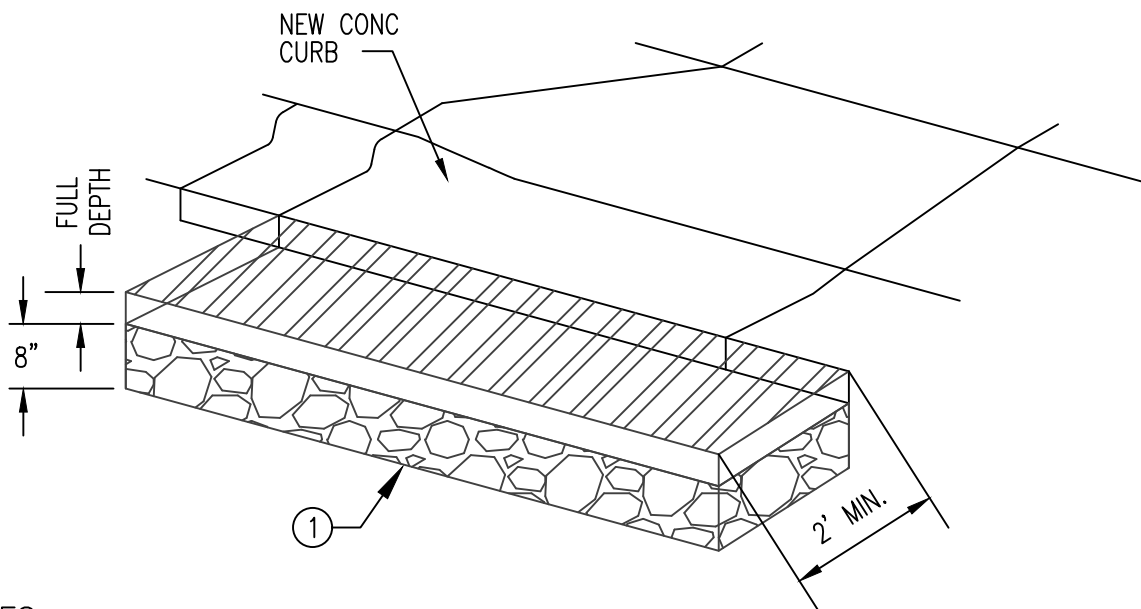
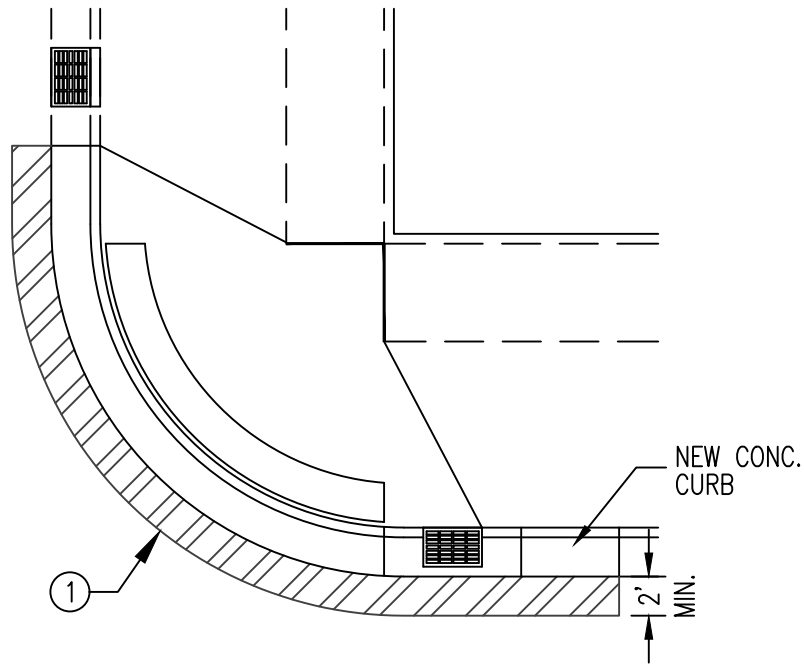
- ① PLACE 4" AGGREGATE BASE BELOW SLAB (INCIDENTAL)
- ② CONCRETE SURFACE SHALL BE BROOM FINISHED
- ③ TIE ALL JOINTS BETWEEN EXPOSED AGGREGATE AND BROOM FINISH CONCRETE, TRANSVERSE AND LONGITUDINAL JOINTS (INCIDENTAL).
4. CONSTRUCT JOINTS WITH APPROVED EXPANSION MATERIAL PER MNDOT TYPE A-E EXPANSION AND SEAL JOINTS WITH SILICONE JOINT AND CRACK SEALER MEETING MN/DOT SPEC 3722 (INCIDENTAL).

DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA

SIDEWALK DETAILS

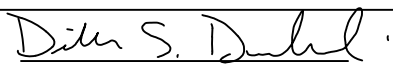
Dimitri S. Dandoul
CITY ENGINEER

SHT 3 OF 3 SHTS	DATE REVISED 1/26/24	PLATE NO. 2-10	REV. N
-----------------	-------------------------	-------------------	-----------



NOTES

- ① THE CONTRACTOR IS RESPONSIBLE FOR SAW CUTTING THE BITUMINOUS PAVEMENT FULL DEPTH A MIN OF 2' FROM NEW LIP OF CURB. CONTRACTOR IS ALSO RESPONSIBLE FOR PLACING AND COMPACTING AGGREGATE BASE.
- 2. THE CONTRACTOR IS RESPONSIBLE TO REMOVE ALL DEBRIS FROM TRENCH AREA, INCLUDING EXCESS CONCRETE, FORMWORK, SPIKES, ETC.
- 3. THE CITY WILL BE RESPONSIBLE FOR PLACING AND COMPACTING THE BITUMINOUS MIX FOR DRIVEWAY REPLACEMENTS, AS SHOWN OR STATED IN CITY BID PROJECTS, OR AS DIRECTED BY THE CITY ENGINEER.

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA BITUMINOUS PATCH FOR CONC. CURB REPLACEMENT			
 CITY ENGINEER			
SHT 1 OF 1 SHTS	DATE REVISED 1/1/23	PLATE NO. 2-11	REV. B

SIDEWALK REPAIR

SIDEWALK REPAIRS ARE REQUIRED BY THE CITY FOR HORIZONTAL AND VERTICAL DISPLACEMENT 1/4" OR GREATER AND FOR SUNKEN OR RAISED SIDEWALK PANELS EXCEEDING 1" FROM ORIGINAL GRADE. GENERAL SURFACE DETERIORATION AND OTHER CONDITIONS MAY ALSO REQUIRE SIDEWALK REPAIRS.

ALIGN REPLACEMENT SECTIONS PARALLEL OR PERPENDICULAR TO EXISTING JOINTS TO MAXIMUM EXTENTS PRACTICABLE.

REPLACE ALL JOINT LINES PER EXISTING JOINT PATTERNS.

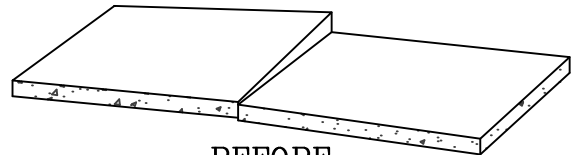
GRINDING OR HORIZONTAL CUTTING

ANY DISPLACEMENT OF MORE THAN 1/4" WILL REQUIRE A RAMP OF 1:12 (8.3%)

GRINDING REPAIR SECTION SHALL NOT EXCEED 1" VERTICALLY.



SECTION



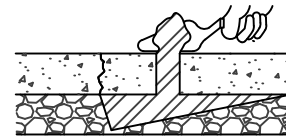
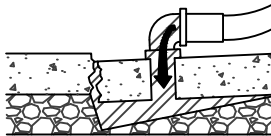
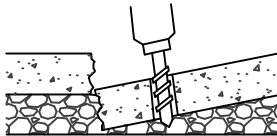
BEFORE



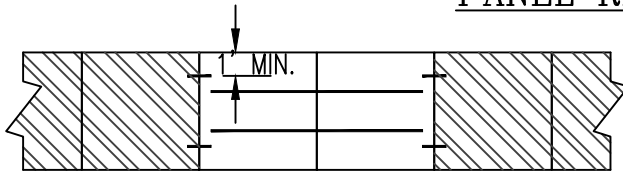
AFTER

SLAB-JACKING

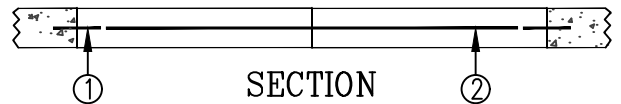
SAND, MUD OR POLY-JACKING IS RECOMMENDED AS A CORRECTIVE MEASURE WHEN THE SIDEWALK IS SUNKEN BY MORE THAN 1/4". CARE SHOULD BE TAKEN TO IDENTIFY THE CAUSE OF THE SETTLEMENT AND TO ENSURE THAT THE ISSUE HAS BEEN ADDRESSED.



PANEL REPLACEMENT



PLAN



SECTION

- ① DRILL & GROUT 12" NO. 4 EPOXY COATED TIE BARS.
- ② SUPPLEMENTAL EPOXY COATED REINFORCEMENT NO. 4 BAR IS REQUIRED FOR REPAIRS OF 3 PANELS OR LESS.
3. SAW CUT CONCRETE REMOVAL WALK FULL DEPTH.
4. SEE STANDARD PLATE 2-10 FOR FURTHER INFORMATION.
5. DRILL & GROUT TIE BARS, TIE BARS, AND SUPPLEMENTAL REINFORCEMENT BARS SHALL BE INCIDENTAL TO PANEL REPLACEMENT.

DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA

SIDEWALK REPAIRS

Dylan S. Dahl
CITY ENGINEER

SHT 1 OF 1 SHTS	DATE REVISED 1/1/23	PLATE NO. 2-12	REV. B
-----------------	------------------------	-------------------	-----------