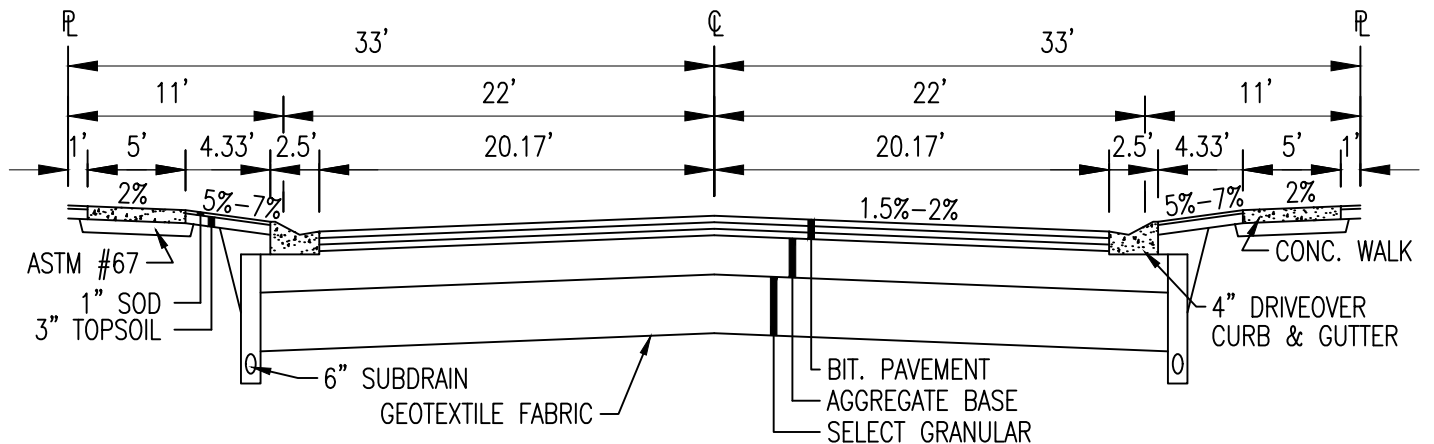


BATTERFACE CURB

CENTERLINE - 20.00' TO LIP OF GUTTER
 CENTERLINE - 22.67' TO BACK OF CURB
 PROP. SIDE SWLK. - 9.33' FROM BACK OF CURB

CENTERLINE IS 0.40' ABOVE LIP OF GUTTER
 CENTERLINE IS 0.03' ABOVE TOP OF CURB
 PROP. SIDE SWLK. IS 0.27' ABOVE TOP OF CURB



DRIVEOVER CURB

CENTERLINE - 20.17' TO LIP OF GUTTER
 CENTERLINE - 22.67' TO BACK OF CURB
 PROP. SIDE SWLK. - 9.33' FROM BACK OF CURB

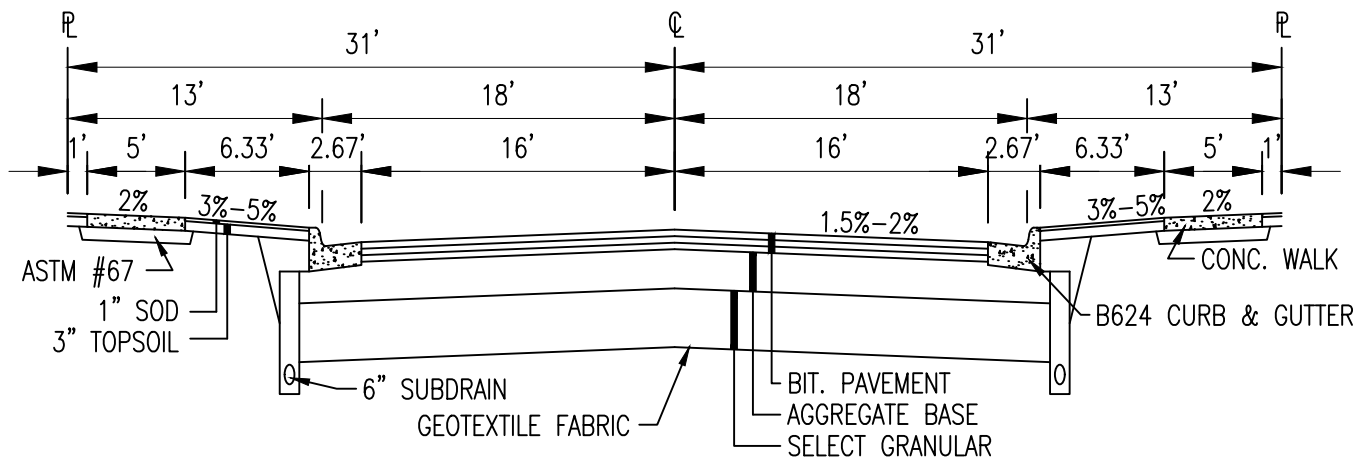
CENTERLINE IS 0.40' ABOVE LIP OF GUTTER
 CENTERLINE IS 0.15' ABOVE TOP OF CURB
 PROP. SIDE SWLK. IS 0.39' ABOVE TOP OF CURB

NOTES

1. MIN. WIDTH FOR PLANTING TREES IS:
7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.

ROCHESTER S.D.P. REF. 1-08, 2-01, 2-10

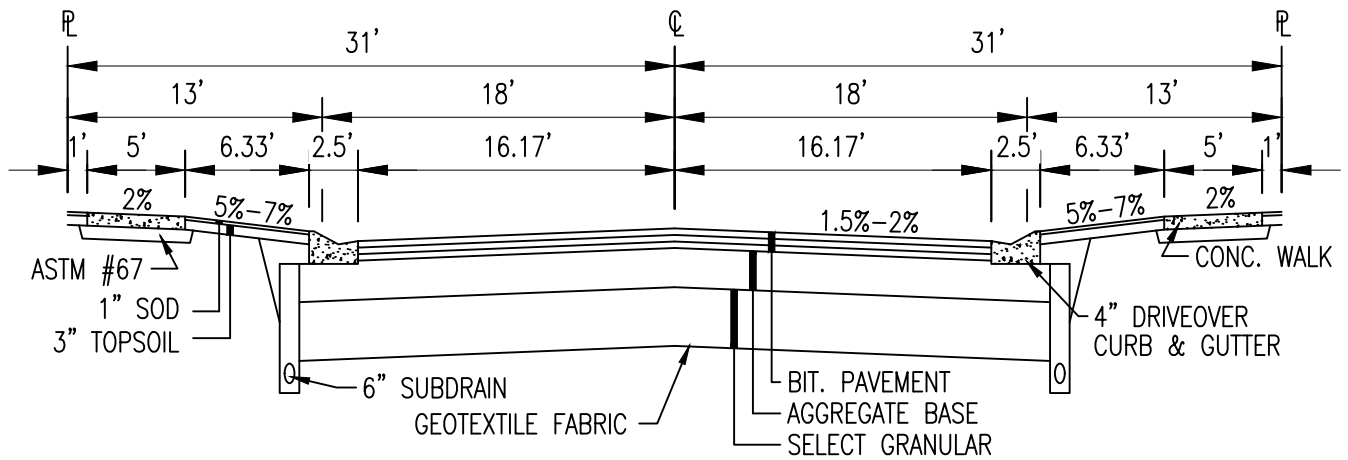
DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA			
ROADWAY TYPICAL SECTION			
44' RDWY. - 66' R.O.W.			
<i>Dylan S. Dahl</i> CITY ENGINEER			
SHT 1 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB

CENTERLINE - 16.00' TO LIP OF GUTTER
 CENTERLINE - 18.67' TO BACK OF CURB
 PROP. SIDE SWLK. - 11.33' FROM BACK OF CURB

CENTERLINE IS 0.32' ABOVE LIP OF GUTTER
 CENTERLINE IS 0.05' BELOW TOP OF CURB
 PROP. SIDE SWLK. IS 0.35' ABOVE TOP OF CURB



DRIVEOVER CURB

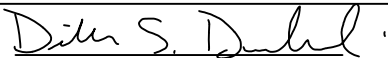
CENTERLINE - 16.17' TO LIP OF GUTTER
 CENTERLINE - 18.67' TO BACK OF CURB
 PROP. SIDE SWLK. - 11.33' FROM BACK OF CURB

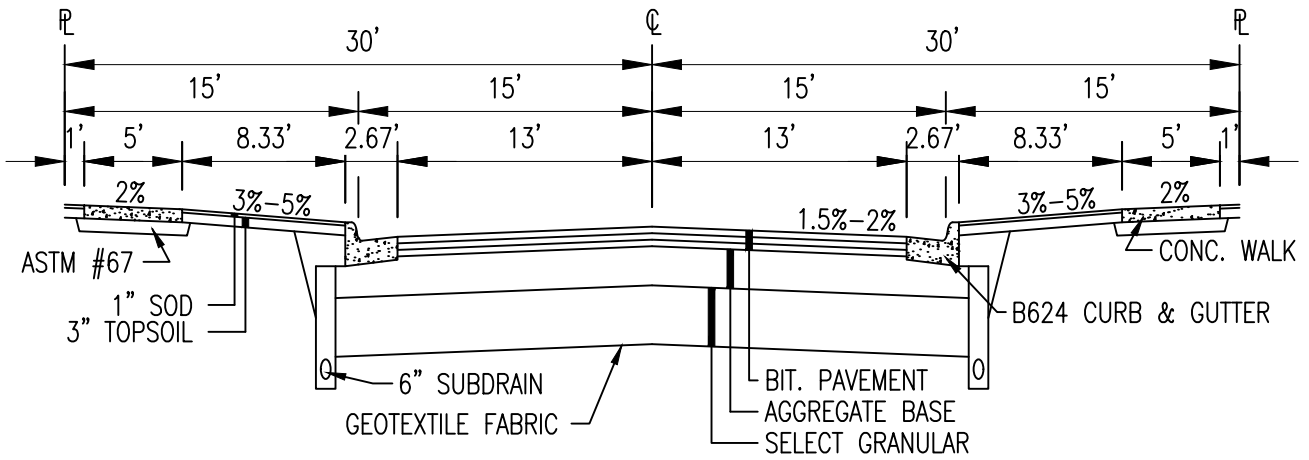
CENTERLINE IS 0.32' ABOVE LIP OF GUTTER
 CENTERLINE IS 0.07' ABOVE TOP OF CURB
 PROP. SIDE SWLK. IS 0.48' ABOVE TOP OF CURB

NOTES

1. MIN. WIDTH FOR PLANTING TREES IS:
7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.

ROCHESTER S.D.P. REF. 1-08, 2-01, 2-10

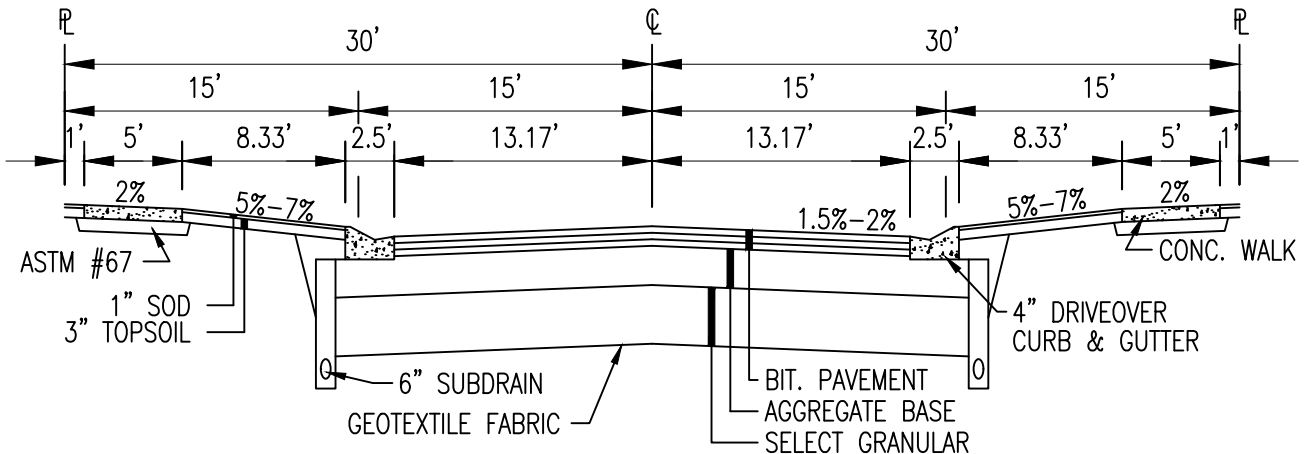
DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA ROADWAY TYPICAL SECTION 36' RDWY. - 62' R.O.W.			
 CITY ENGINEER			
SHT 3 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB

CENTERLINE - 13.00' TO LIP OF GUTTER
 CENTERLINE - 15.67' TO BACK OF CURB
 PROP. SIDE SWLK. - 13.33' FROM BACK OF CURB

CENTERLINE IS 0.26' ABOVE LIP OF GUTTER
 CENTERLINE IS 0.11' BELOW TOP OF CURB
 PROP. SIDE SWLK. IS 0.43' ABOVE TOP OF CURB



DRIVEOVER CURB

CENTERLINE - 13.17' TO LIP OF GUTTER
 CENTERLINE - 15.67' TO BACK OF CURB
 PROP. SIDE SWLK. - 13.33' FROM BACK OF CURB

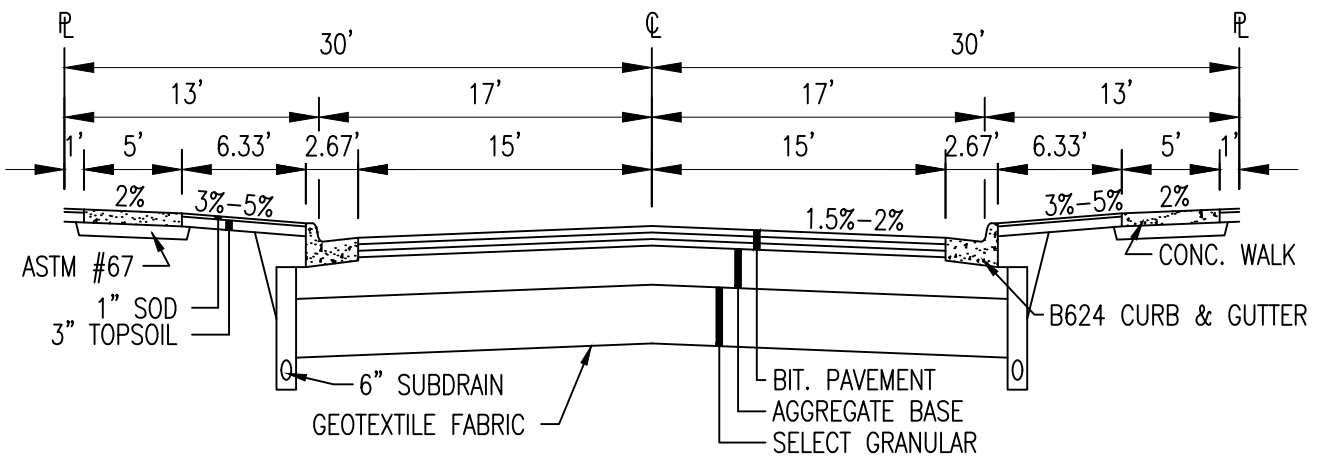
CENTERLINE IS 0.26' ABOVE LIP OF GUTTER
 CENTERLINE IS 0.01' ABOVE TOP OF CURB
 PROP. SIDE SWLK. IS 0.56' ABOVE TOP OF CURB

NOTES

1. MIN. WIDTH FOR PLANTING TREES IS:
7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.

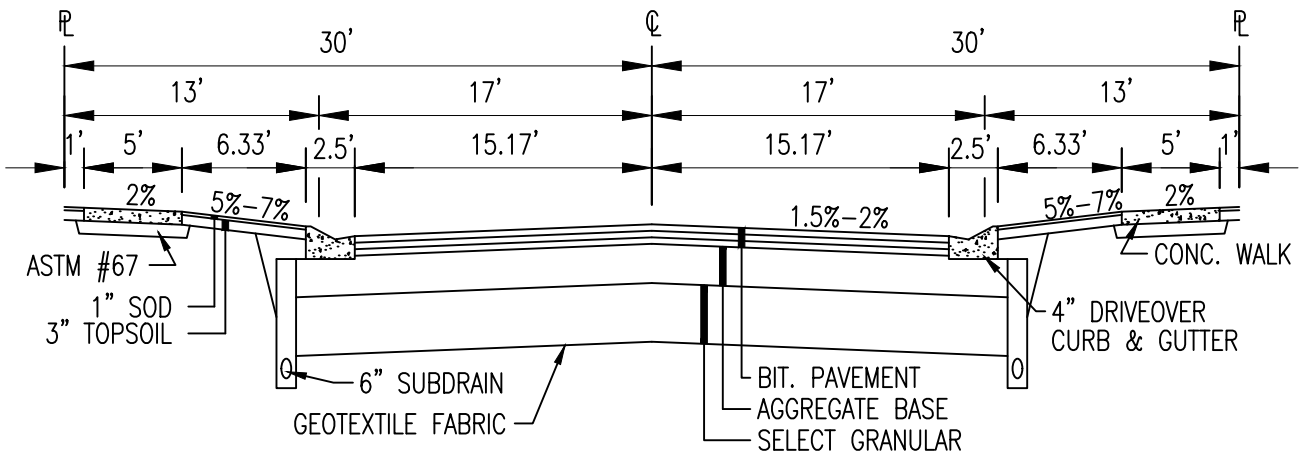
ROCHESTER S.D.P. REF. 1-08, 2-01, 2-10

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA			
ROADWAY TYPICAL SECTION			
30' RDWY. - 60' R.O.W.			
<i>Dale S. Dahl</i> CITY ENGINEER			
SHT 4 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB

CENTERLINE - 15.00' TO LIP OF GUTTER CENTERLINE IS 0.30' ABOVE LIP OF GUTTER
 CENTERLINE - 17.67' TO BACK OF CURB CENTERLINE IS 0.07' BELOW TOP OF CURB
 PROP. SIDE SWLK. - 11.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.35' ABOVE TOP OF CURB



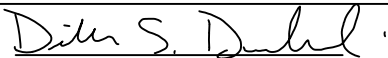
DRIVEOVER CURB

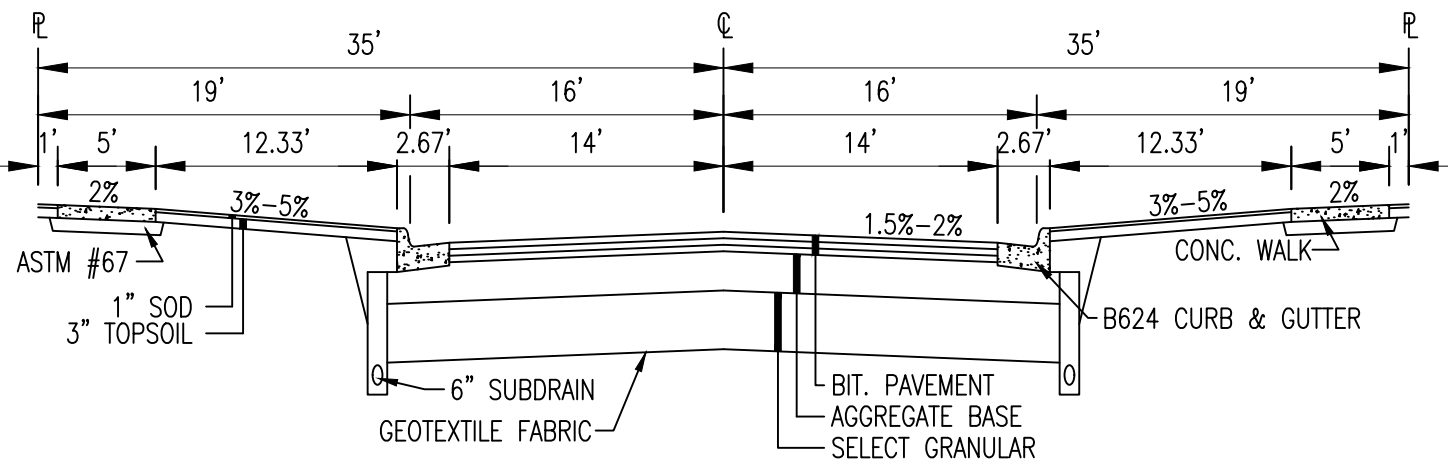
CENTERLINE - 15.17' TO LIP OF GUTTER CENTERLINE IS 0.30' ABOVE LIP OF GUTTER
 CENTERLINE - 17.67' TO BACK OF CURB CENTERLINE IS 0.05' ABOVE TOP OF CURB
 PROP. SIDE SWLK. - 11.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.48' ABOVE TOP OF CURB

NOTES

1. MIN. WIDTH FOR PLANTING TREES IS:
7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.

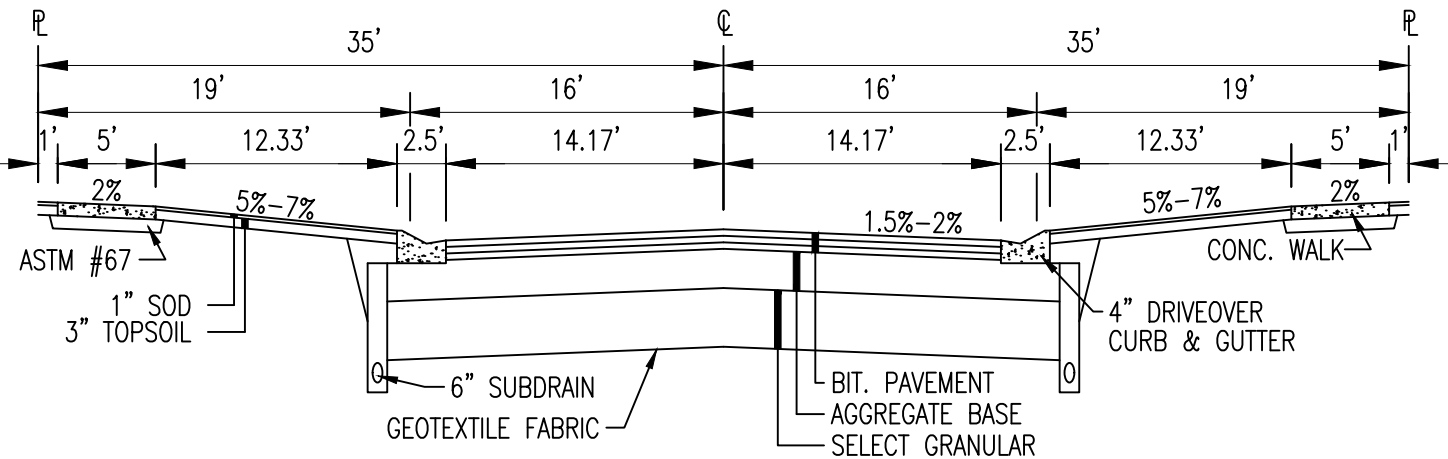
ROCHESTER S.D.P. REF. 1-08, 2-01, 2-10

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA ROADWAY TYPICAL SECTION 34' RDWY. - 60' R.O.W.			
 CITY ENGINEER			
SHT 5 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB

CENTERLINE - 14.00' TO LIP OF GUTTER CENTERLINE IS 0.28' ABOVE LIP OF GUTTER
 CENTERLINE - 16.67' TO BACK OF CURB CENTERLINE IS 0.09' BELOW TOP OF CURB
 PROP. SIDE SWLK. - 17.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.59' ABOVE TOP OF CURB



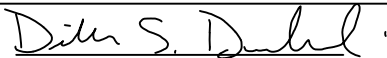
DRIVEOVER CURB

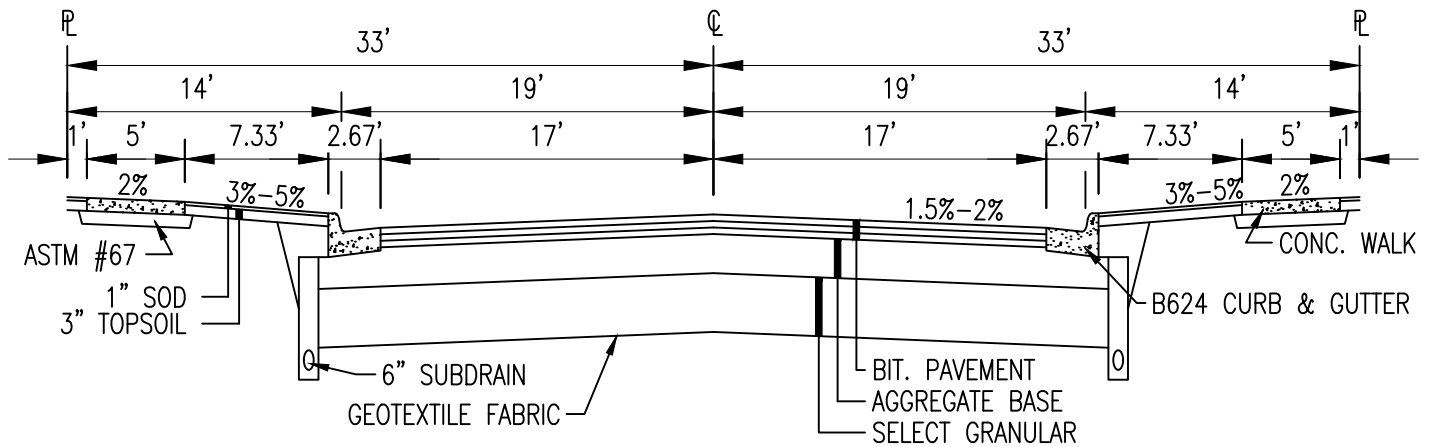
CENTERLINE - 14.17' TO LIP OF GUTTER CENTERLINE IS 0.28' ABOVE LIP OF GUTTER
 CENTERLINE - 16.67' TO BACK OF CURB CENTERLINE IS 0.03' ABOVE TOP OF CURB
 PROP. SIDE SWLK. - 17.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.72' ABOVE TOP OF CURB

NOTES

1. MIN. WIDTH FOR PLANTING TREES IS:
7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.

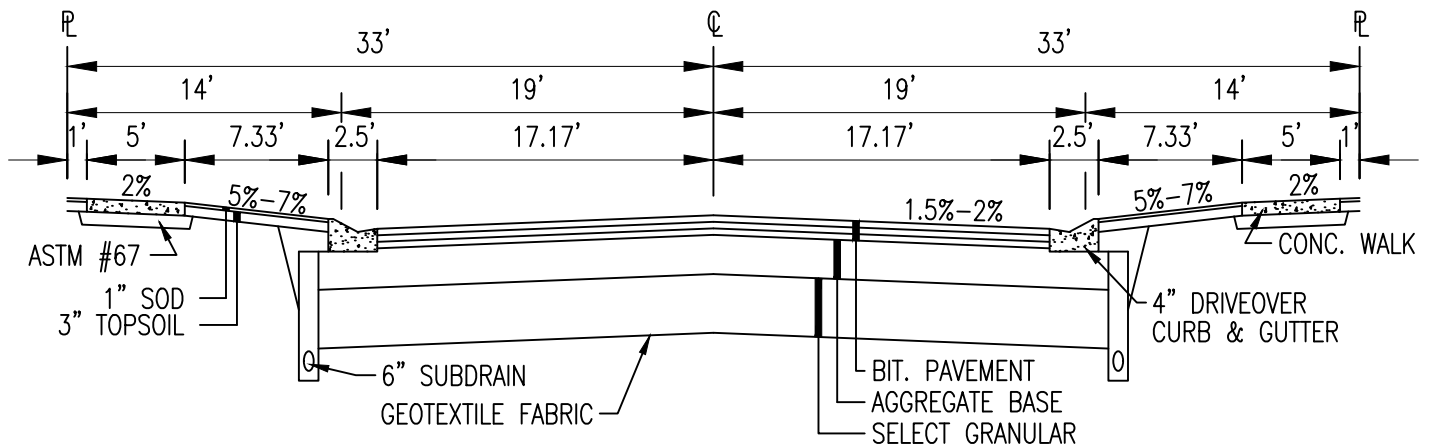
ROCHESTER S.D.P. REF. 1-08, 2-01, 2-10

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA ROADWAY TYPICAL SECTION 32' RDWY. - 70' R.O.W.			
 CITY ENGINEER			
SHT 6 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB

CENTERLINE - 17.00' TO LIP OF GUTTER CENTERLINE IS 0.34' ABOVE LIP OF GUTTER
 CENTERLINE - 19.67' TO BACK OF CURB CENTERLINE IS 0.03' BELOW TOP OF CURB
 PROP. SIDE SWLK. - 12.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.39' ABOVE TOP OF CURB



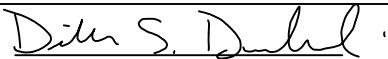
DRIVEOVER CURB

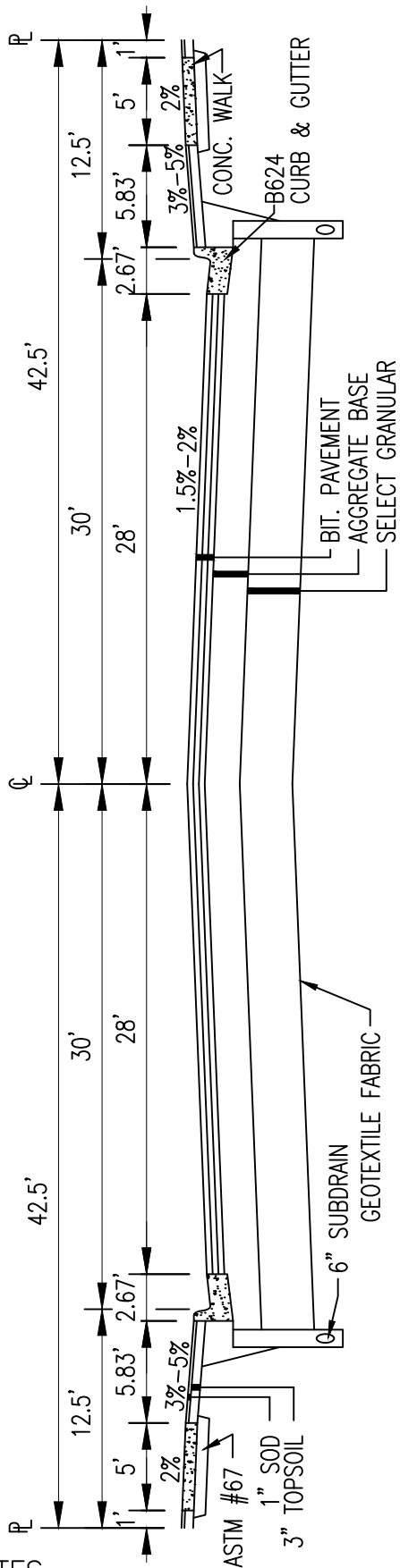
CENTERLINE - 17.17' TO LIP OF GUTTER CENTERLINE IS 0.34' ABOVE LIP OF GUTTER
 CENTERLINE - 19.67' TO BACK OF CURB CENTERLINE IS 0.09' ABOVE TOP OF CURB
 PROP. SIDE SWLK. - 12.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.52' ABOVE TOP OF CURB

NOTES

1. MIN. WIDTH FOR PLANTING TREES IS:
7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.

ROCHESTER S.D.P. REF. 1-08, 2-01, 2-10

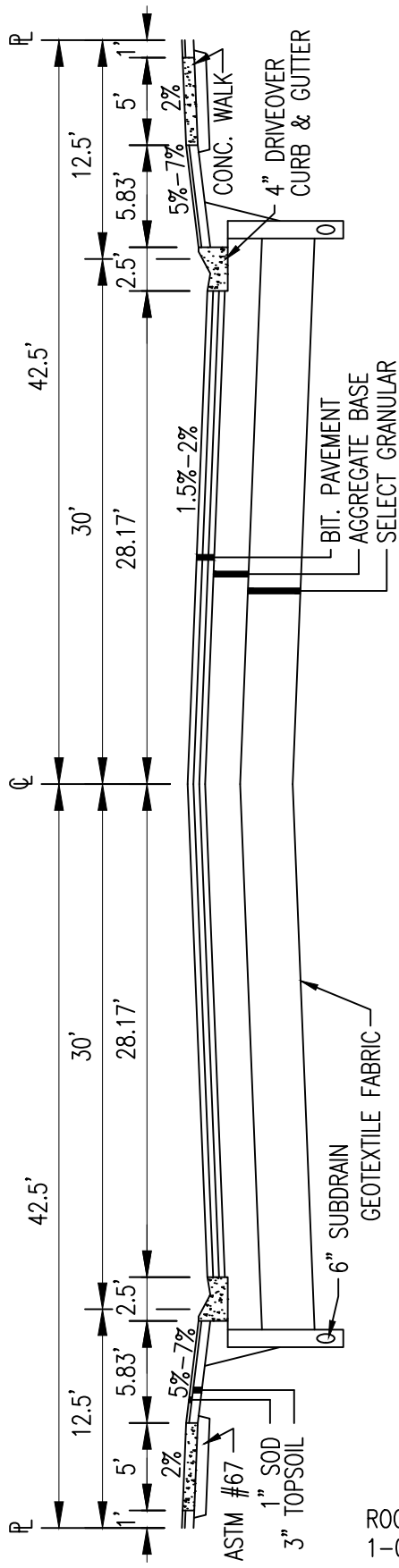
DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA ROADWAY TYPICAL SECTION 38' RDWY. - 66' R.O.W.			
 CITY ENGINEER			
SHT 7 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB

CENTERLINE - 28.00' TO LIP OF GUTTER
 CENTERLINE - 30.67' TO BACK OF CURB
 PROP. SIDE SWLK. - 10.83' FROM BACK OF CURB

CENTERLINE IS 0.56' ABOVE LIP OF GUTTER
 CENTERLINE IS 0.19' ABOVE TOP OF CURB
 PROP. SIDE SWLK. IS 0.33' ABOVE TOP OF CURB



DRIVEOVER CURB

CENTERLINE - 28.17' TO LIP OF GUTTER
 CENTERLINE - 30.67' TO BACK OF CURB
 PROP. SIDE SWLK. - 10.83' FROM BACK OF CURB

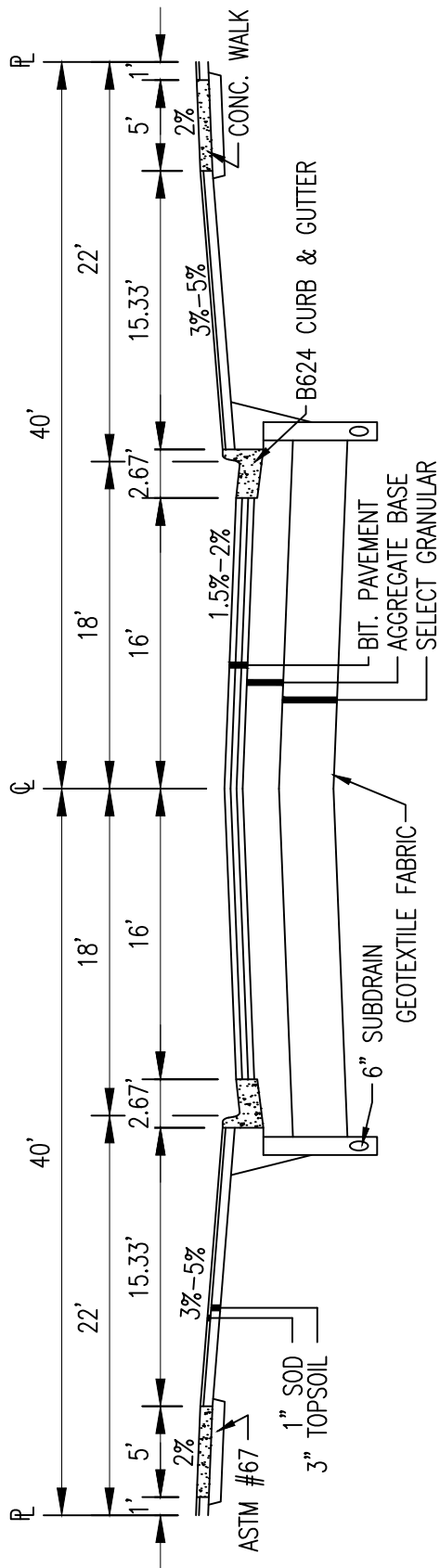
CENTERLINE IS 0.56' ABOVE LIP OF GUTTER
 CENTERLINE IS 0.31' ABOVE TOP OF CURB
 PROP. SIDE SWLK. IS 0.46' ABOVE TOP OF CURB

ROCHESTER S.D.P. REF.
 1-08, 2-01, 2-10

NOTES

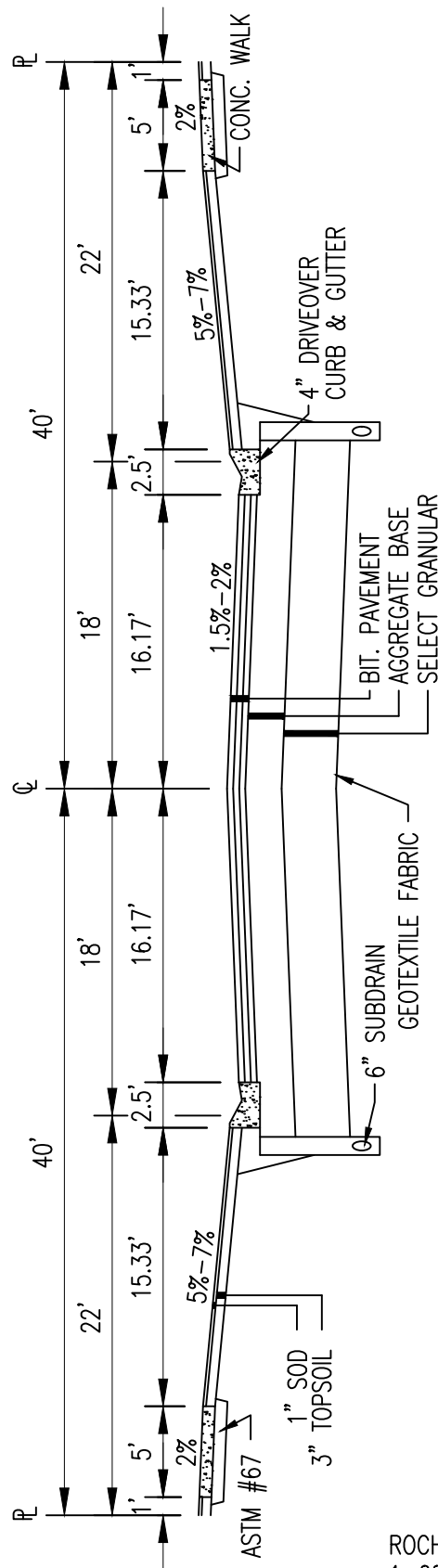
1. MIN. WIDTH FOR PLANTING TREES IS 7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA			
ROADWAY TYPICAL SECTION			
60' RDWY. - 85' R.O.W.			
<i>Diana S. Dahl</i> CITY ENGINEER			
SHT 8 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB

CENTERLINE - 16.00' TO LIP OF GUTTER CENTERLINE IS 0.32' ABOVE LIP OF GUTTER
 CENTERLINE - 18.67' TO BACK OF CURB CENTERLINE IS 0.05' BELOW TOP OF CURB
 PROP. SIDE SWLK. - 20.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.71' ABOVE TOP OF CURB



DRIVEOVER CURB

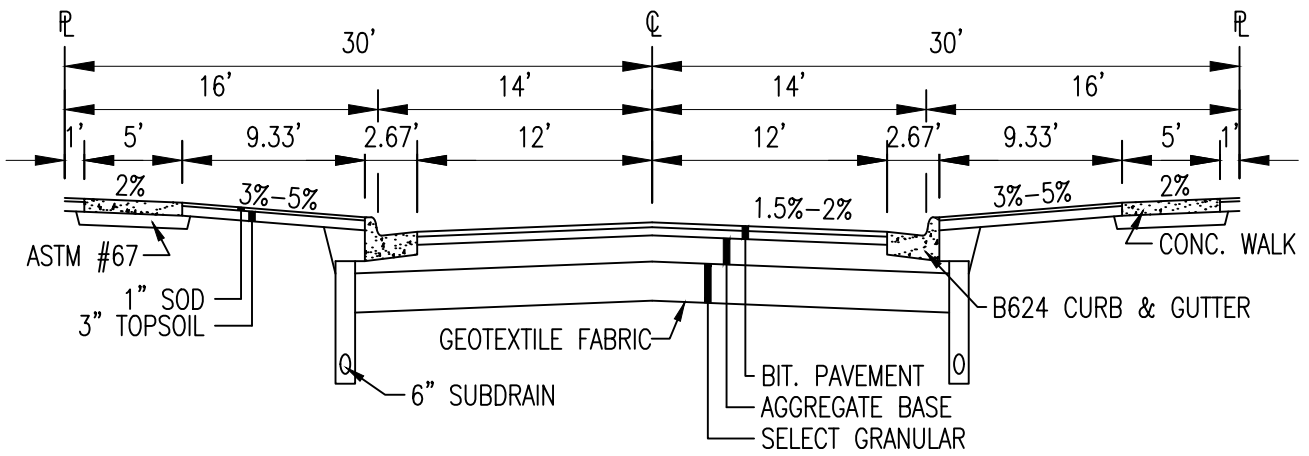
CENTERLINE - 16.17' TO LIP OF GUTTER CENTERLINE IS 0.32' ABOVE LIP OF GUTTER
 CENTERLINE - 18.67' TO BACK OF CURB CENTERLINE IS 0.07' ABOVE TOP OF CURB
 PROP. SIDE SWLK. - 20.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.84' ABOVE TOP OF CURB

ROCHESTER S.D.P. REF.
 1-08, 2-01, 2-10

NOTES

1. MIN. WIDTH FOR PLANTING TREES IS 7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.

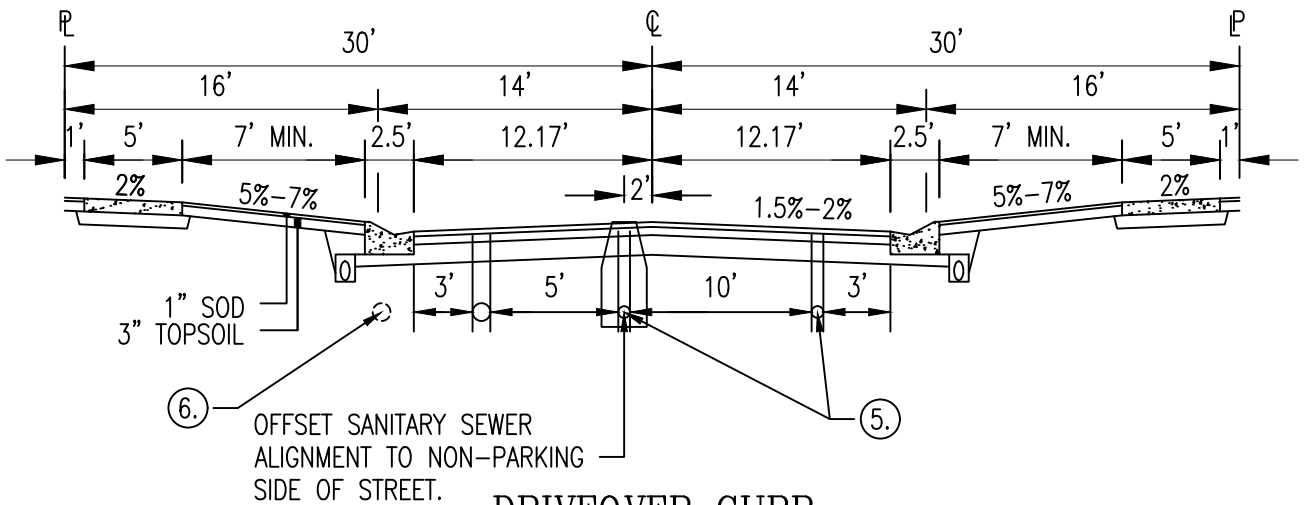
DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA ROADWAY TYPICAL SECTION 36' RDWY. - 80' R.O.W.			
 CITY ENGINEER			
SHT 9 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB

CENTERLINE - 12.00' TO LIP OF GUTTER
 CENTERLINE - 14.67' TO BACK OF CURB
 PROP. SIDE SWLK. - 14.33' FROM BACK OF CURB

CENTERLINE IS 0.24' ABOVE LIP OF GUTTER
 CENTERLINE IS 0.13' BELOW TOP OF CURB
 PROP. SIDE SWLK. IS 0.47' ABOVE TOP OF CURB



DRIVEOVER CURB

CENTERLINE - 12.17' TO LIP OF GUTTER
 CENTERLINE - 14.67' TO BACK OF CURB
 PROP. SIDE SWLK. - 14.33' FROM BACK OF CURB

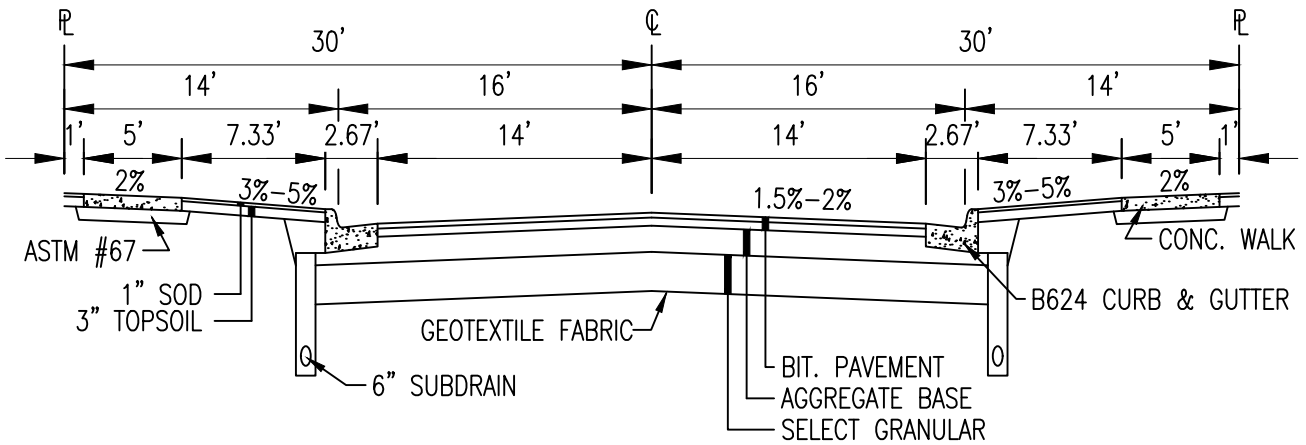
CENTERLINE IS 0.24' ABOVE LIP OF GUTTER
 CENTERLINE IS 0.01' BELOW TOP OF CURB
 PROP. SIDE SWLK. IS 0.60' ABOVE TOP OF CURB

NOTES

1. MIN. WIDTH FOR PLANTING TREES IS:
7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.
4. REFER TO ENGINEERING SPECIFICATIONS SEC 1002.1 FOR DETAILS ON RIGHT OF WAY & STREET WIDTHS.
- ⑤. REFER TO ENGINEERING SPECIFICATIONS SEC 1002.17 FOR DETAILS ON GENERAL PLACEMENT OF UTILITIES.
- ⑥. REFER TO ENGINEERING SPECIFICATIONS SEC 1005.6 FOR DETAILS ON UTILITY PLACEMENT WITHIN 28' ROADWAY SECTION.

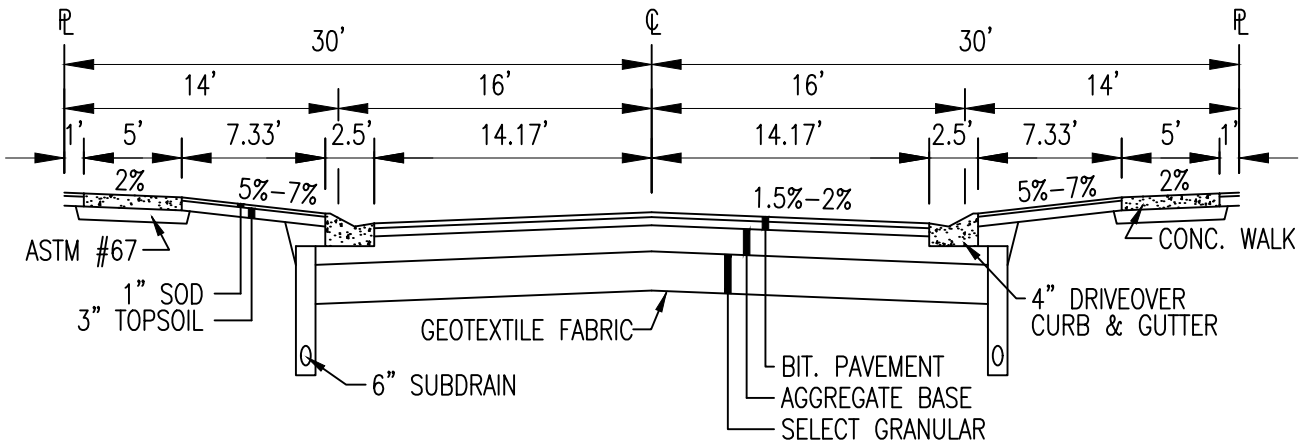
ROCHESTER S.D.P. REF.
1-08, 2-01, 2-10

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA ROADWAY TYPICAL SECTION 28' RDWY. - 60' R.O.W.			
 CITY ENGINEER			
SHT 10 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB

CENTERLINE - 14.00' TO LIP OF GUTTER CENTERLINE IS 0.28' ABOVE LIP OF GUTTER
 CENTERLINE - 16.67' TO BACK OF CURB CENTERLINE IS 0.09' BELOW TOP OF CURB
 PROP. SIDE SWLK. - 12.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.39' ABOVE TOP OF CURB



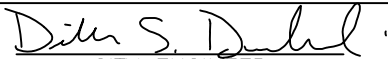
DRIVEOVER CURB

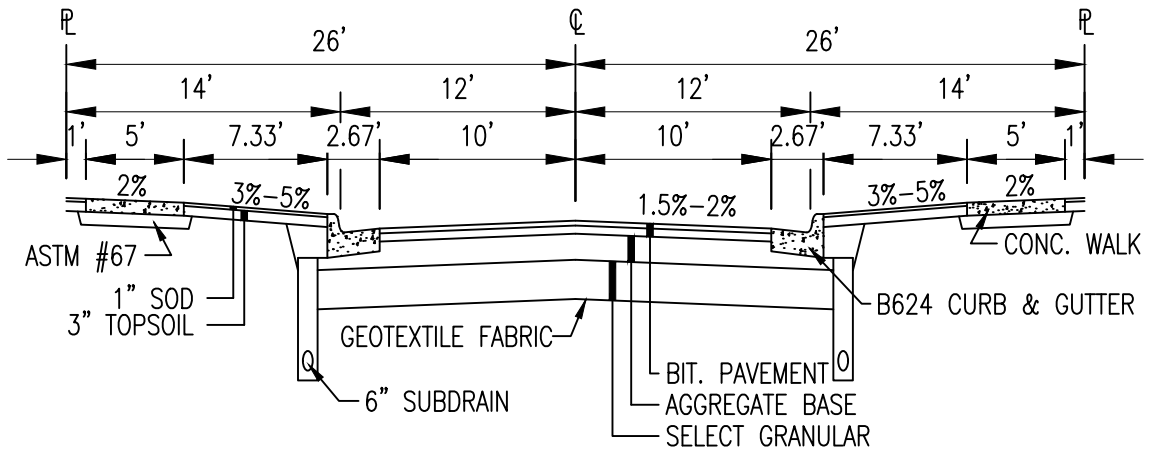
CENTERLINE - 14.17' TO LIP OF GUTTER CENTERLINE IS 0.28' ABOVE LIP OF GUTTER
 CENTERLINE - 16.67' TO BACK OF CURB CENTERLINE IS 0.03' ABOVE TOP OF CURB
 PROP. SIDE SWLK. - 12.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.51' ABOVE TOP OF CURB

NOTES

1. MIN. WIDTH FOR PLANTING TREES IS:
7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.

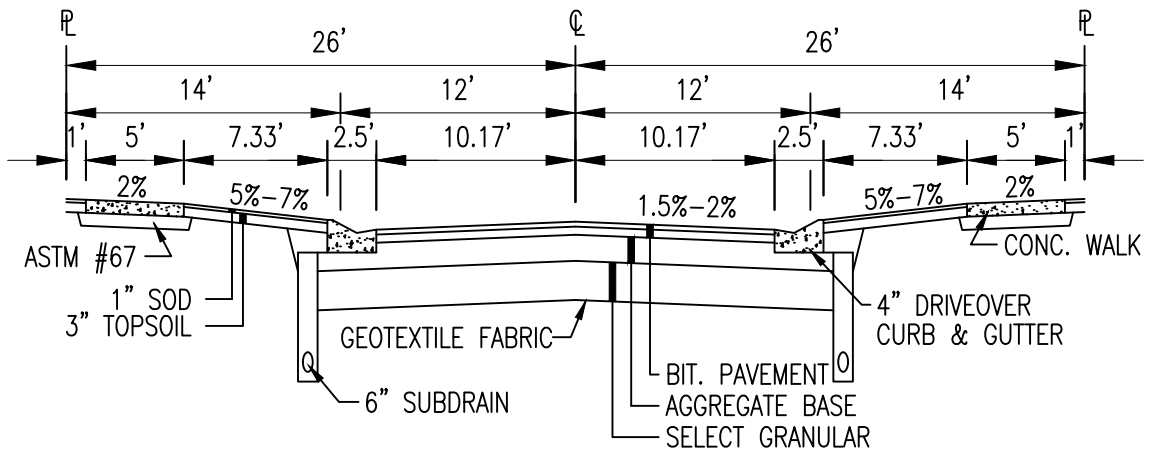
ROCHESTER S.D.P. REF.
1-08, 2-01, 2-10

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA ROADWAY TYPICAL SECTION 32' RDWY. - 60' R.O.W.			
 CITY ENGINEER			
SHT 11 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB

CENTERLINE - 10.00' TO LIP OF GUTTER CENTERLINE IS 0.20' ABOVE LIP OF GUTTER
 CENTERLINE - 12.67' TO BACK OF CURB CENTERLINE IS 0.17' BELOW TOP OF CURB
 PROP. SIDE SWLK. - 12.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.39' ABOVE TOP OF CURB



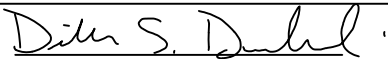
DRIVEOVER CURB

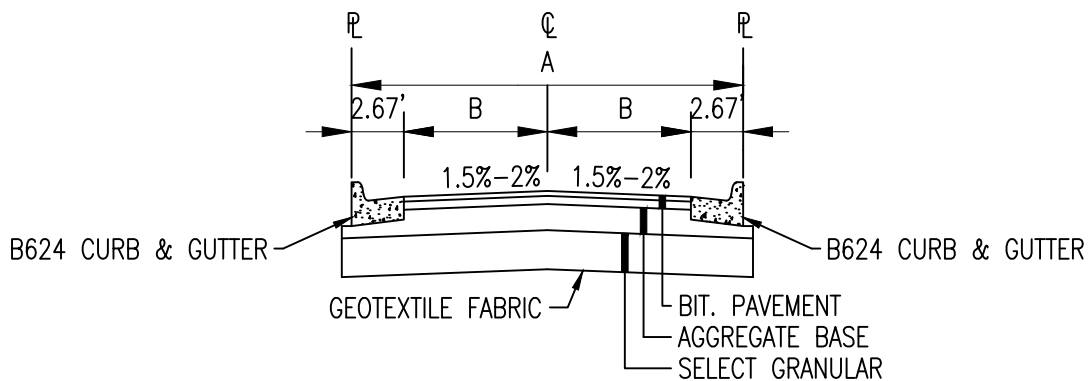
CENTERLINE - 10.17' TO LIP OF GUTTER CENTERLINE IS 0.20' ABOVE LIP OF GUTTER
 CENTERLINE - 12.67' TO BACK OF CURB CENTERLINE IS 0.05' BELOW TOP OF CURB
 PROP. SIDE SWLK. - 12.33' FROM BACK OF CURB PROP. SIDE SWLK. IS 0.51' ABOVE TOP OF CURB

NOTES

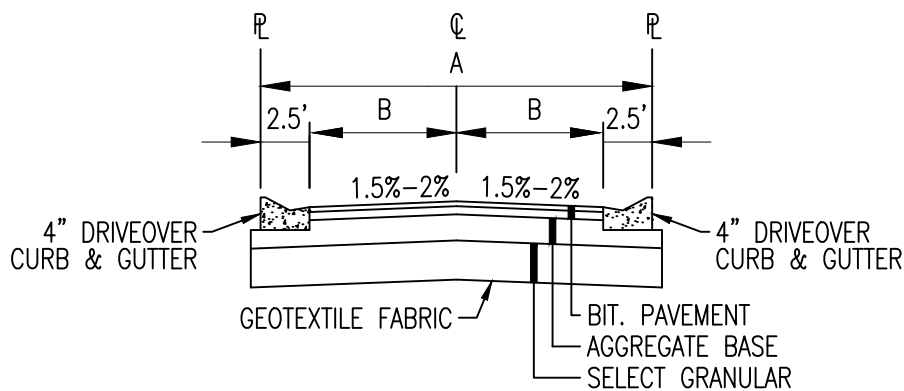
1. MIN. WIDTH FOR PLANTING TREES IS:
7' FOR BOULEVARDS
2. REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
3. AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.
4. CITY ENGINEER APPROVAL REQUIRED FOR ROADWAY DESIGNS LESS THAN 28'.

ROCHESTER S.D.P. REF.
1-08, 2-01, 2-10

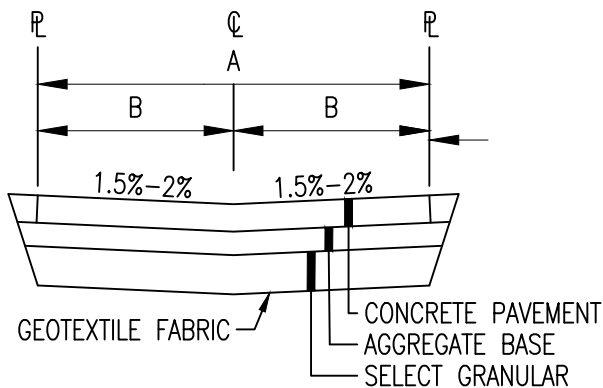
DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA ROADWAY TYPICAL SECTION 24' RDWY. - 52' R.O.W.			
 CITY ENGINEER			
SHT 12 OF 13 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-01	REV. 1



BATTERFACE CURB



DRIVEOVER CURB



INVERTED CROWN

DISTRICT	ALLEY TYPE	A (MIN)	B (MIN)	CENTERLINE TO LIP ELEVATION	CENTERLINE TO TBC ELEVATION	CENTERLINE TO EDGE ELEVATION
RESIDENTIAL	BATTERFACE CURB	16'	5.33'	+0.11'	-0.26'	
RESIDENTIAL	DRIVEOVER CURB	16'	5.5'	+0.11'	-0.14'	
RESIDENTIAL	INVERTED CROWN	16'	8'			-0.16'
MIXED USE, IND.	BATTERFACE CURB	18' *	6.33' *	+0.13' *	-0.24' *	
MIXED USE, IND.	DRIVEOVER CURB	18' *	6.5' *	+0.13' *	-0.12' *	
MIXED USE, IND.	INVERTED CROWN	18' *	9' *			-0.18' *

*UNLESS 20' MINIMUM REQUIRED FOR FIRE EQUIPMENT ACCESS

ROCHESTER STANDARD DETAIL PLATE REF. 2-01

NOTES

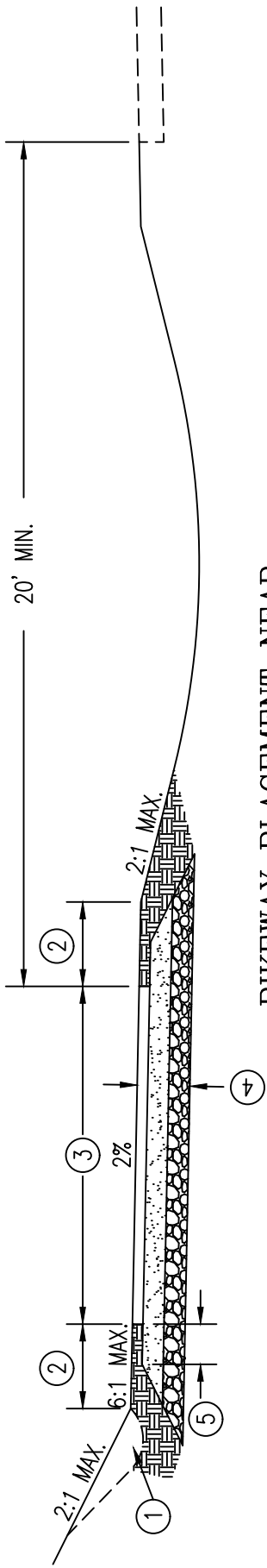
- REFER TO ENGINEERING STANDARDS FOR PAVEMENT DESIGN.
- AFTER COMPACTION, THE FINAL LIFT WEAR ADJACENT TO CONCRETE PAVEMENTS MUST BE SLIGHTLY HIGHER BUT NOT TO EXCEED 1/4" HIGHER THAN THE CONCRETE SURFACE.

DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA

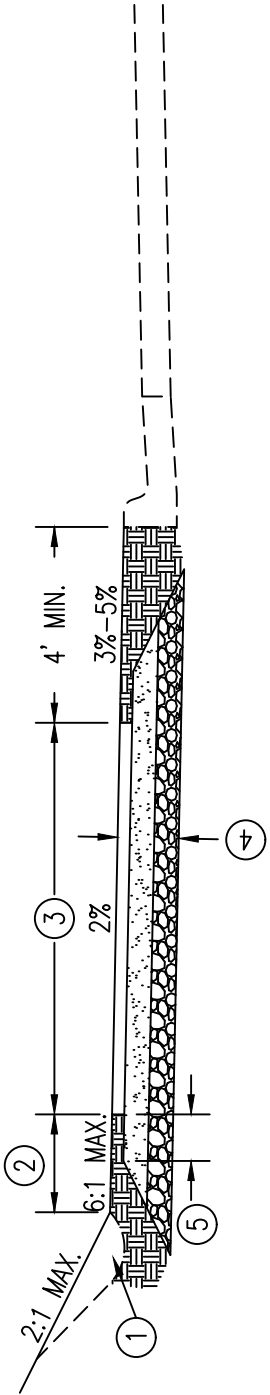
ALLEY TYPICAL SECTION

Dylan S. Dahl
CITY ENGINEER

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BIKEWAY PLACEMENT NEAR
ROADS WITHOUT CURBS



BIKEWAY PLACEMENT NEAR
ROADS WITH CURBS

NOTES

- ① DITCH IF REQUIRED FOR DRAINAGE.
- ② GRADED AREA ADJACENT TO THE PAVED SURFACE SHALL BE A MINIMUM OF 2' AND FREE OF OBSTRUCTIONS.
- ③ MINIMUM 10' WIDTH FOR 2 WAY TRAFFIC, (5' MINIMUM FOR 1 WAY). FOR CURVES WITH LESS THAN 50' RADIUS, PROVIDE ADDITIONAL WIDTH AS PER TABLE.
- ④ 3" LIFT BITUMINOUS SPWEA230B WEARING COURSE
6" AGGREGATE BASE TYPICAL
STABILIZING AGGREGATE AS DIRECTED BY THE ENGINEER
- ⑤ 1'-2' BEYOND PAVEMENT

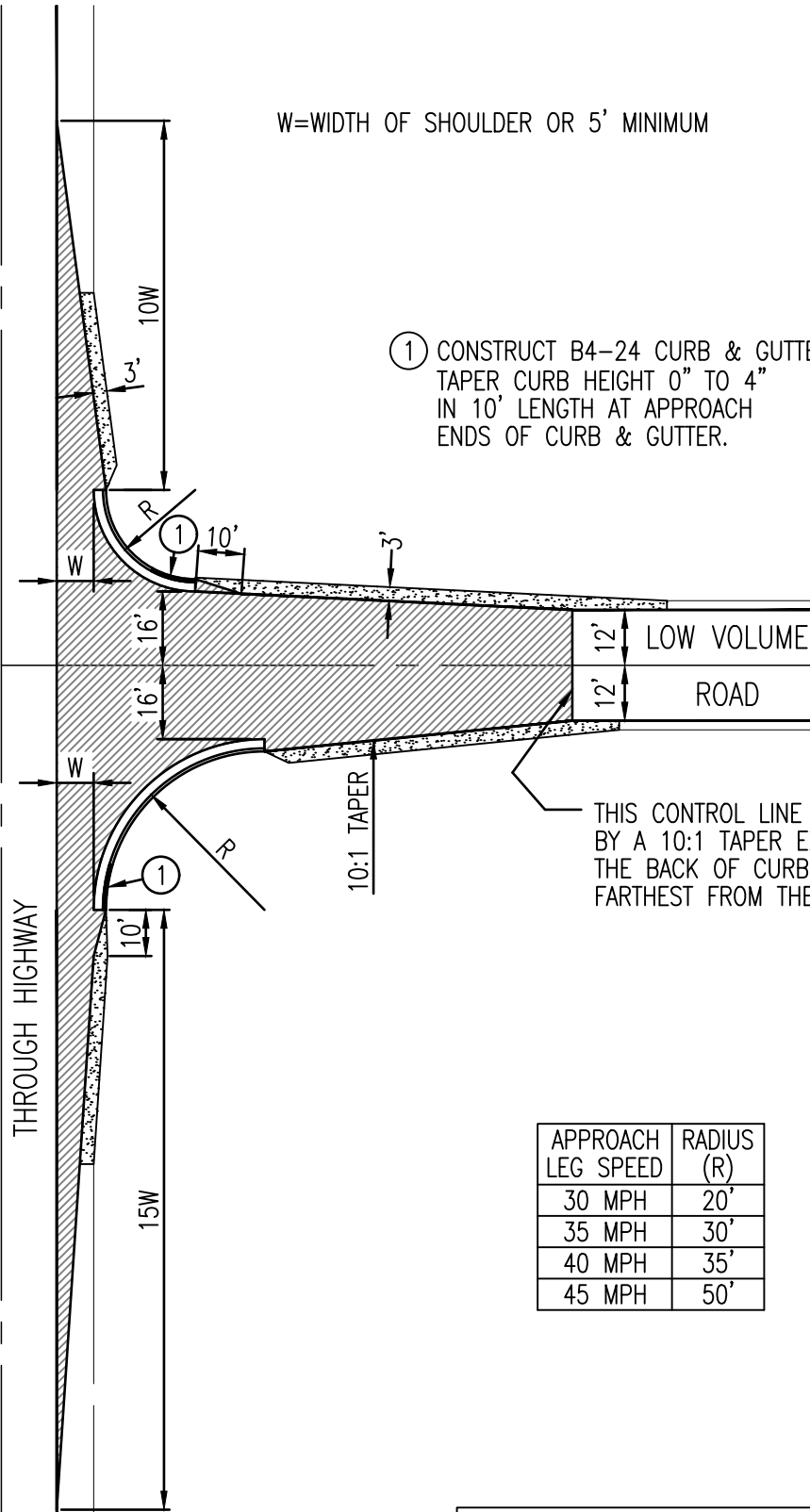
RADIUS	ADDITIONAL PAVED WIDTH
0-25	2'
25-50	1'

DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA
**TYPICAL SECTION
OFF-ROAD BIKEWAY**

Dylan S. Dahl
CITY ENGINEER

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W=WIDTH OF SHOULDER OR 5' MINIMUM



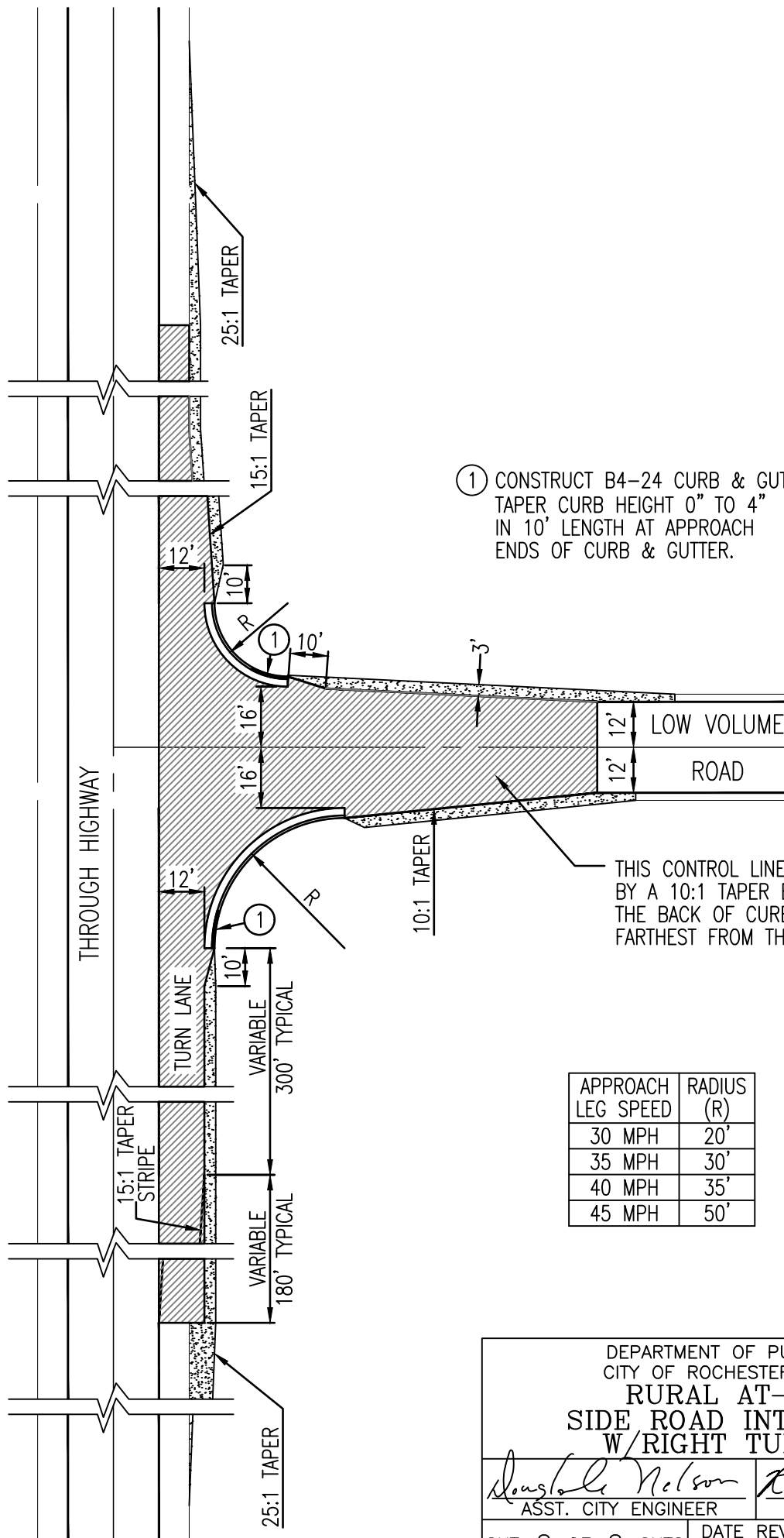
APPROACH LEG SPEED	RADIUS (R)
30 MPH	20'
35 MPH	30'
40 MPH	35'
45 MPH	50'

DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA

**RURAL AT-GRADE
SIDE ROAD INTERSECTION**

Douglas Nelson ASST. CITY ENGINEER *Keith W. Freme* DIRECTOR

SHT 1 OF 2 SHTS	DATE REVISED 3/22/06	PLATE NO. 3-03	REV. A
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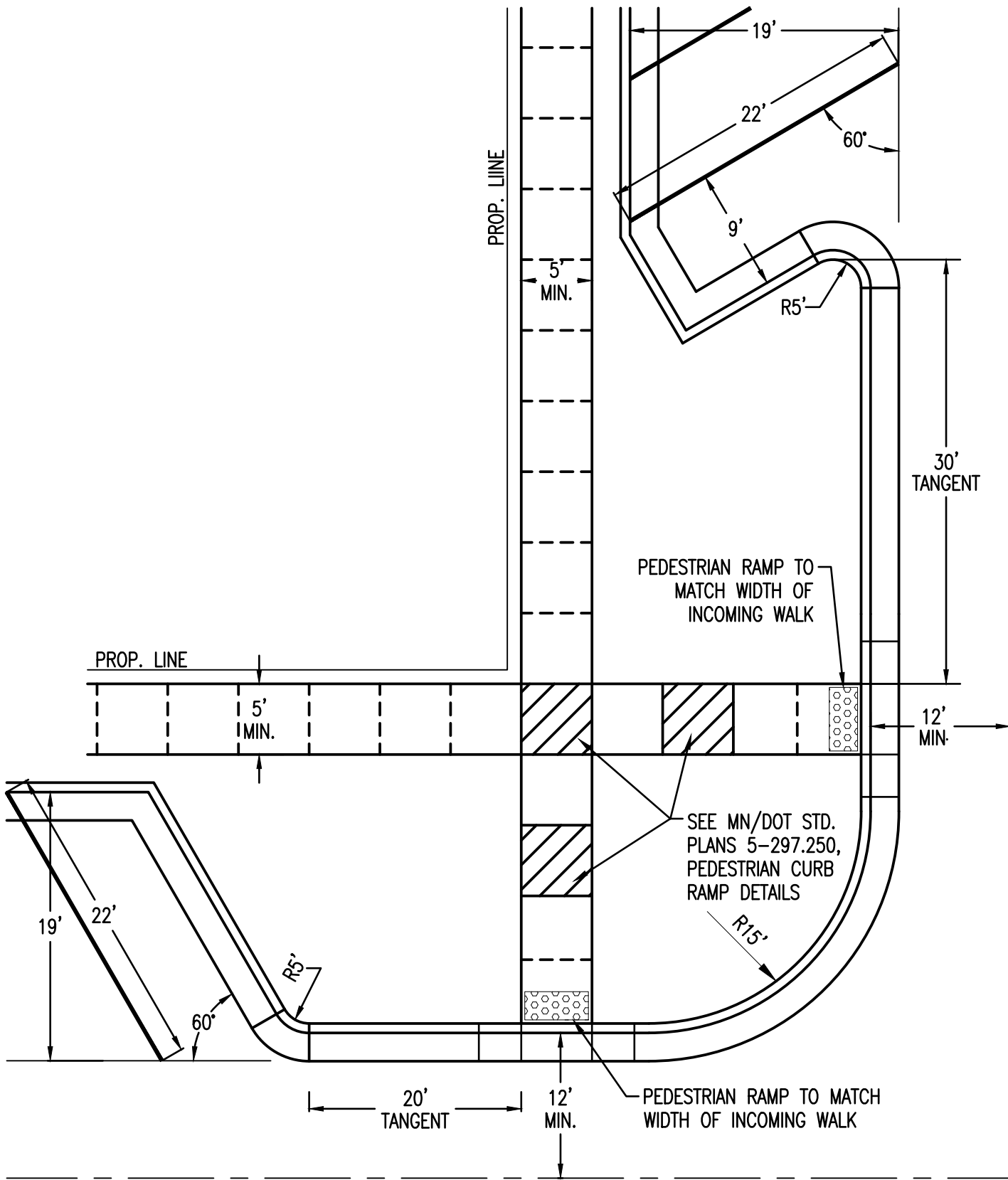
① CONSTRUCT B4-24 CURB & GUTTER TAPER CURB HEIGHT 0" TO 4" IN 10' LENGTH AT APPROACH ENDS OF CURB & GUTTER.

THIS CONTROL LINE IS ESTABLISHED BY A 10:1 TAPER EXTENDED FROM THE BACK OF CURB END LOCATED FARTHEST FROM THE THROUGH HIGHWAY.

APPROACH LEG SPEED	RADIUS (R)
30 MPH	20'
35 MPH	30'
40 MPH	35'
45 MPH	50'

DEPARTMENT OF PUBLIC WORKS
 CITY OF ROCHESTER, MINNESOTA
**RURAL AT-GRADE
 SIDE ROAD INTERSECTION
 W/RIGHT TURN LANE**

<i>Douglas Nelson</i> ASST. CITY ENGINEER	<i>Keith W. Freme</i> DIRECTOR
SHT 2 OF 2 SHTS	DATE REVISED 3/22/06
	PLATE NO. 3-03
	REV. A

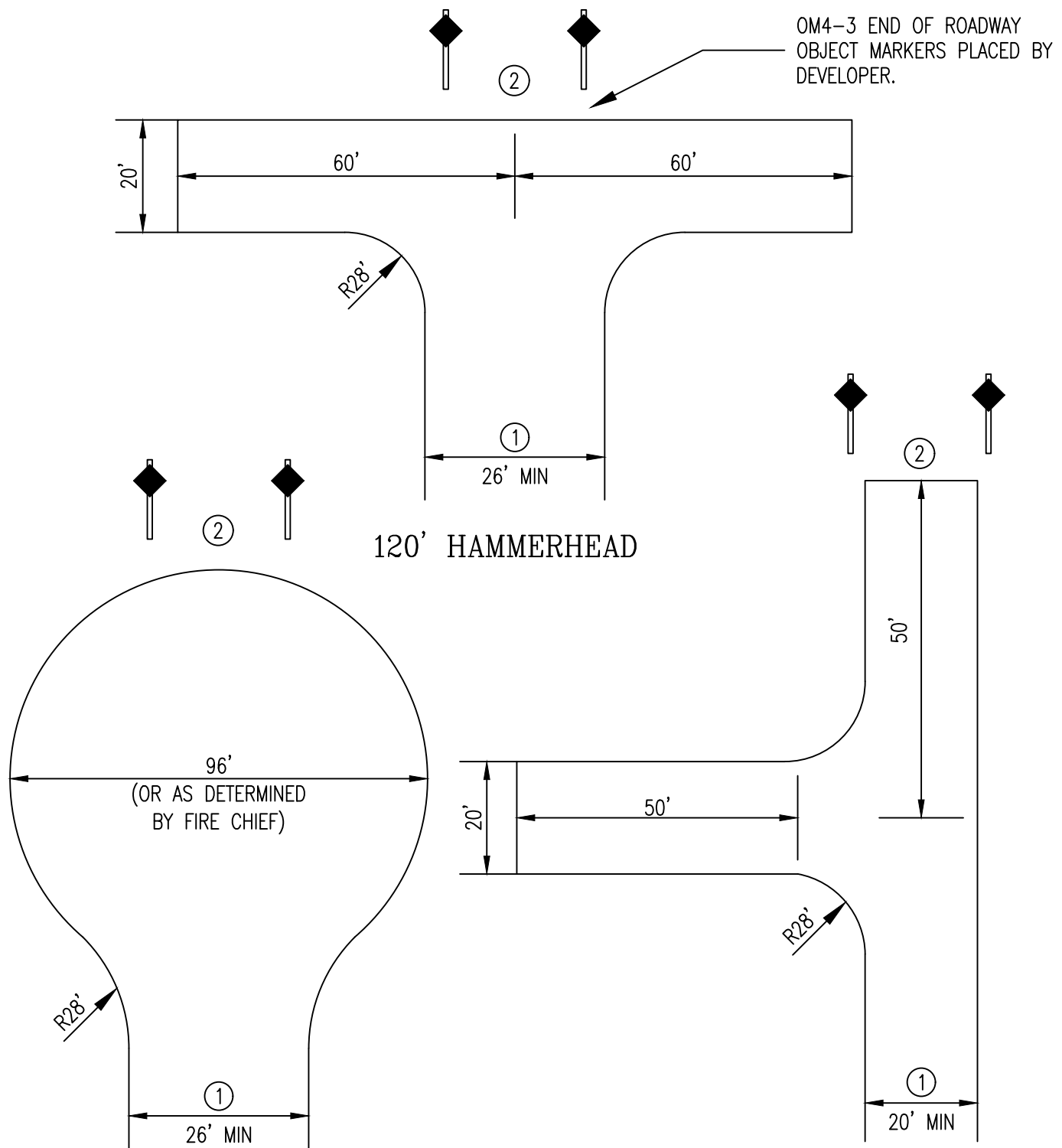


DEPARTMENT OF PUBLIC WORKS
 CITY OF ROCHESTER, MINNESOTA
BUMP-OUT DETAILS
ANGLE PARKING

Dustin S. Dahl
 CITY ENGINEER

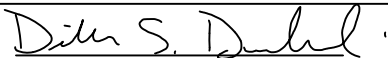
SEE S.D.P.2-10 FOR SIDEWALK CONSTRUCTIONS DETAILS

SHT 2 OF 2 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-04	REV. C
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NOTES

- ① MATCH EXISTING STREET WIDTH AND PAVEMENT SECTION.
- ② TWO MnMUTCD COMPLIANT OM4-3 END OF ROADWAY OBJECT MARKERS TO BE FULLY REFLECTORIZED AND PROPERLY MAINTAINED UNTIL THE STREET IS EXTENDED.

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA			
TEMPORARY STREET ENDS			
 CITY ENGINEER			
SHT 1 OF 2 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-05	REV. E

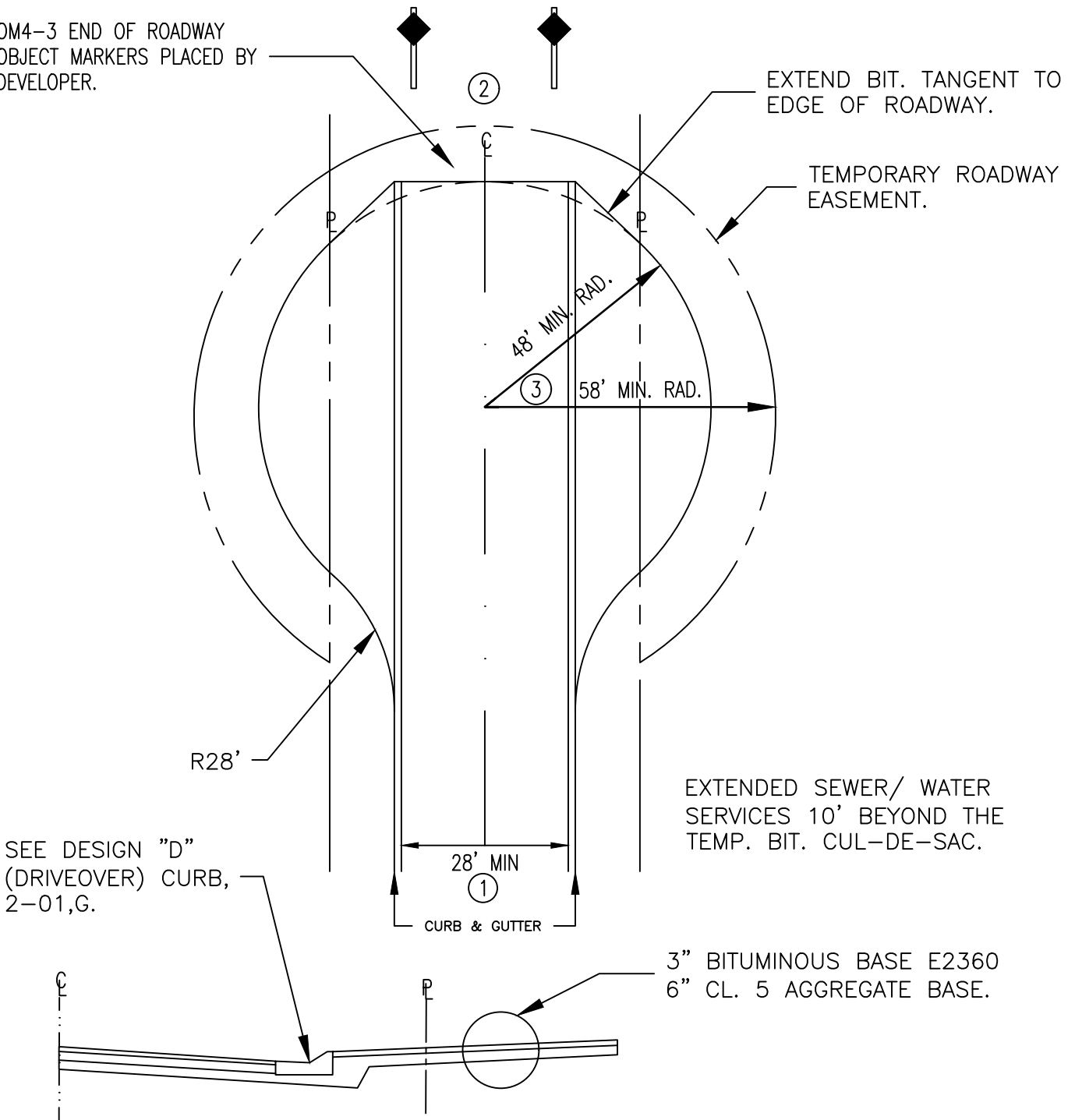
FUTURE SEWER AND WATER LINES ARE TO BE EXTENDED BEYOND THE END OF THE STREET.

A HYDRANT & GATE VALVE SHALL BE PLACED AT THE END OF THE WATERMAIN.

OM4-3 END OF ROADWAY OBJECT MARKERS PLACED BY DEVELOPER.

EXTEND BIT. TANGENT TO EDGE OF ROADWAY.

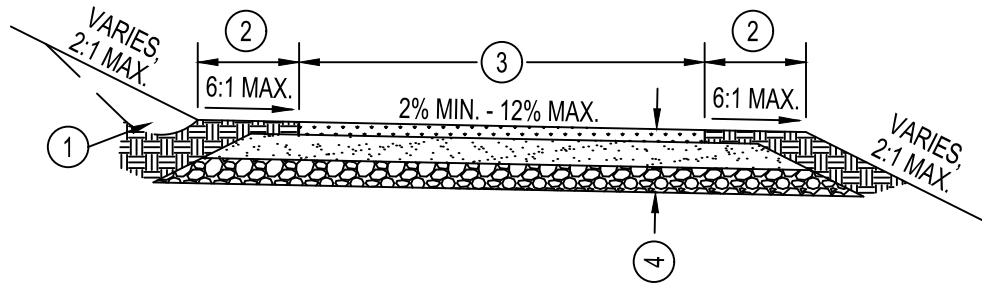
TEMPORARY ROADWAY EASEMENT.



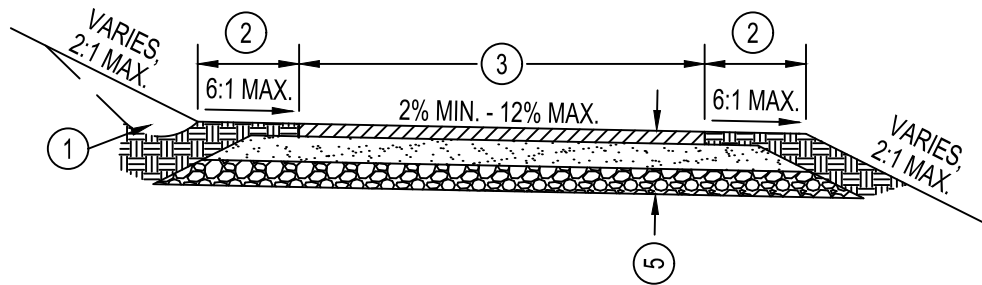
NOTES

- ① MATCH EXISTING STREET WIDTH AND PAVEMENT SECTION.
- ② TWO MnMUTCD COMPLIANT OM4-3 END OF ROADWAY OBJECT MARKERS TO BE FULLY REFLECTORIZED AND PROPERLY MAINTAINED UNTIL THE STREET IS EXTENDED.
- ③ MINIMUM RADII AS NOTED, OR AS DETERMINED BY FIRE CHIEF.

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA			
TEMPORARY CUL-DE-SAC			
<i>Dustin S. Dahl</i> CITY ENGINEER			
SHT 2 OF 2 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-05	REV. E



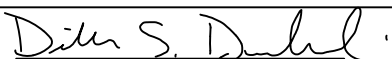
TOPSOIL SEEDED SANITARY ACCESS ROAD



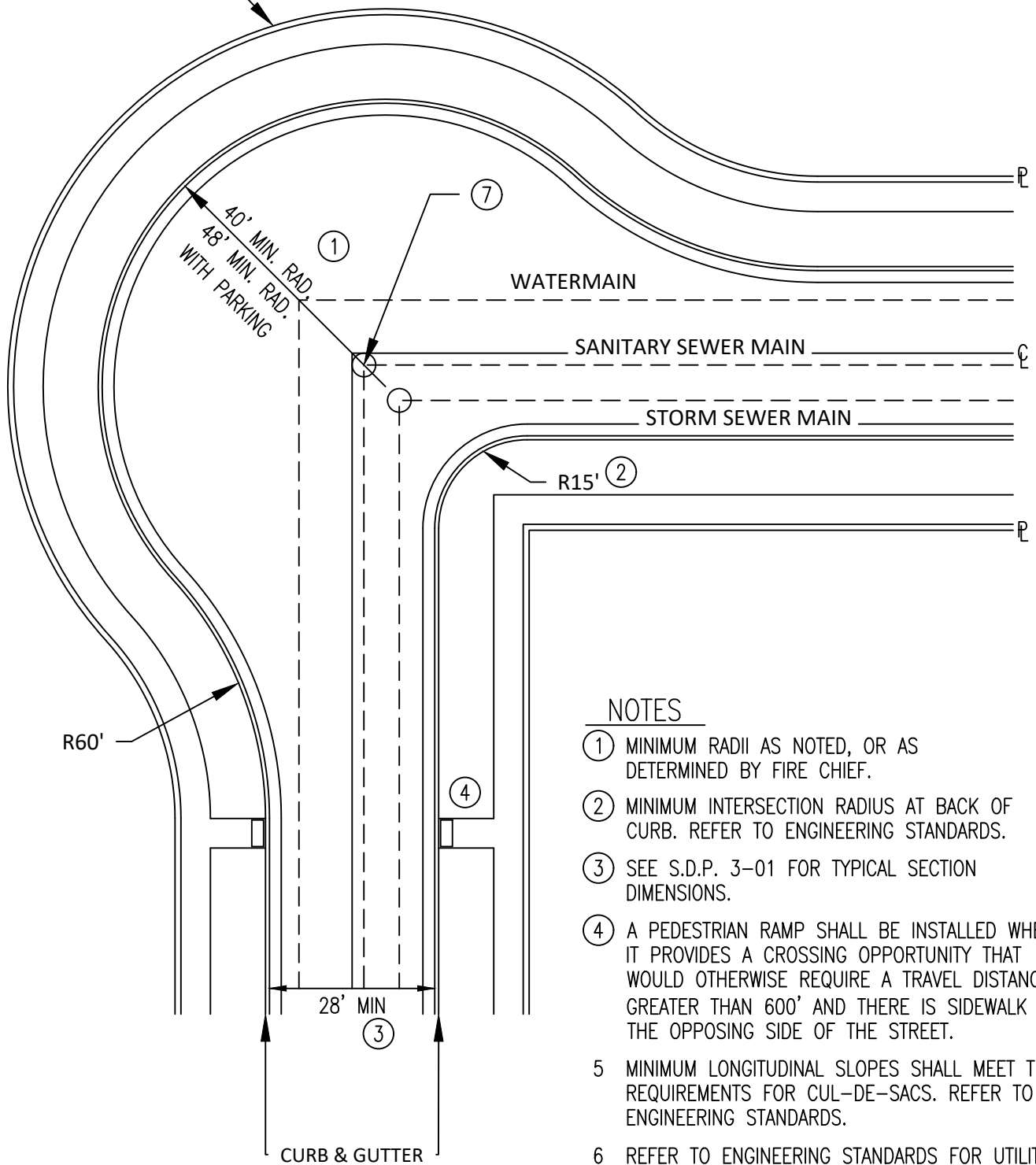
PAVED SANITARY ACCESS ROAD
(REQUIRED FOR LIFT STATIONS)

NOTES

- ① DITCH IF REQUIRED FOR DRAINAGE.
- ② GRADED AREA ADJACENT TO THE ACCESS ROAD TO BE A MINIMUM OF 2' AND FREE OF OBSTRUCTIONS.
- ③ MINIMUM 14' WIDTH
- ④ 4" TOPSOIL, SITE RESTORATION TYPE 3 OR 6
6" AGGREGATE BASE TYPICAL
STABILIZING AGGREGATE AS DIRECTED BY THE ENGINEER.
- ⑤ 4" LIFT BITUMINOUS SPWEA230B WEARING COURSE
6" AGGREGATE BASE TYPICAL
STABILIZING AGGREGATE AS DIRECTED BY THE ENGINEER.

DEPARTMENT OF PUBLIC WORKS CITY OF ROCHESTER, MINNESOTA SANITARY ACCESS ROAD TYPICAL SECTIONS			
 CITY ENGINEER			
SHT 1 OF 1 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-06	REV. A

PROPERTY LINE



NOTES

- ① MINIMUM RADII AS NOTED, OR AS DETERMINED BY FIRE CHIEF.
- ② MINIMUM INTERSECTION RADIUS AT BACK OF CURB. REFER TO ENGINEERING STANDARDS.
- ③ SEE S.D.P. 3-01 FOR TYPICAL SECTION DIMENSIONS.
- ④ A PEDESTRIAN RAMP SHALL BE INSTALLED WHEN IT PROVIDES A CROSSING OPPORTUNITY THAT WOULD OTHERWISE REQUIRE A TRAVEL DISTANCE GREATER THAN 600' AND THERE IS SIDEWALK ON THE OPPOSING SIDE OF THE STREET.
- 5 MINIMUM LONGITUDINAL SLOPES SHALL MEET THE REQUIREMENTS FOR CUL-DE-SACS. REFER TO ENGINEERING STANDARDS.
- 6 REFER TO ENGINEERING STANDARDS FOR UTILITY PLACEMENT.
- ⑦ CENTER OF KNUCKLE RADIUS SHALL BE PLACED BETWEEN CENTERLINE AND INSIDE LIP OF CURB.

DEPARTMENT OF PUBLIC WORKS
CITY OF ROCHESTER, MINNESOTA

KNUCKLE CORNER

Dylan S. Dahl
CITY ENGINEER

SHT 1 OF 1 SHTS	DATE REVISED 2/2/26	PLATE NO. 3-07	REV. A
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